

STANDARD DRAWING LEGEND FOR ENTIRE PLAN SET														
LIMIT OF WORK	LOW	LOW												
LIMIT OF DISTURBANCE	LOD	LOD												
EXISTING NOTE	TYPICAL NOTE TEXT	PROPOSED NOTE												
---	ON-SITE PROPERTY LINE / R.O.W. LINE	---												
---	NEIGHBORING PROPERTY LINE / INTERIOR PARCEL LINE	---												
---	EASEMENT LINE	---												
---	SETBACK LINE	---												
<table border="1"> <thead> <tr> <th colspan="3">CONCRETE CURB & GUTTER</th> </tr> </thead> <tbody> <tr> <td>---</td> <td>CURB AND GUTTER</td> <td>---</td> </tr> <tr> <td>---</td> <td>SPILL TRANSITION</td> <td>---</td> </tr> <tr> <td>---</td> <td>DEPRESSED CURB AND GUTTER</td> <td>---</td> </tr> </tbody> </table>			CONCRETE CURB & GUTTER			---	CURB AND GUTTER	---	---	SPILL TRANSITION	---	---	DEPRESSED CURB AND GUTTER	---
CONCRETE CURB & GUTTER														
---	CURB AND GUTTER	---												
---	SPILL TRANSITION	---												
---	DEPRESSED CURB AND GUTTER	---												
---	UTILITY POLE WITH LIGHT	---												
---	POLE LIGHT	---												
---	TRAFFIC LIGHT	---												
---	UTILITY POLE	---												
---	TYPICAL LIGHT	---												
---	ACORN LIGHT	---												
---	TYPICAL SIGN	---												
---	PARKING COUNTS	---												
---	CONTOUR LINE	---												
---	SPOT ELEVATIONS	---												
---	SANITARY LABEL	---												
---	STORM LABEL	---												
---	SANITARY SEWER LATERAL	---												
---	UNDERGROUND WATER LINE	---												
---	UNDERGROUND ELECTRIC LINE	---												
---	UNDERGROUND GAS LINE	---												
---	OVERHEAD WIRE	---												
---	UNDERGROUND TELEPHONE LINE	---												
---	UNDERGROUND CABLE LINE	---												
---	STORM SEWER	---												
---	SANITARY SEWER MAIN	---												
---	HYDRANT	---												
---	SANITARY MANHOLE	---												
---	STORM MANHOLE	---												
---	WATER METER	---												
---	WATER VALVE	---												
---	GAS VALVE	---												
---	GAS METER	---												
---	TYPICAL END SECTION	---												
---	HEADWALL OR ENDWALL	---												
---	GRATE INLET	---												
---	CURB INLET	---												
---	CLEAN OUT	---												
---	ELECTRIC MANHOLE	---												
---	TELEPHONE MANHOLE	---												
---	ELECTRIC BOX	---												
---	ELECTRIC PEDESTAL	---												
---	MONITORING WELL	---												
---	TEST PIT	---												
---	BENCHMARK	---												
---	BORING	---												

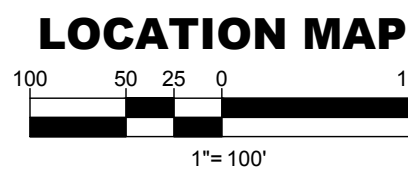
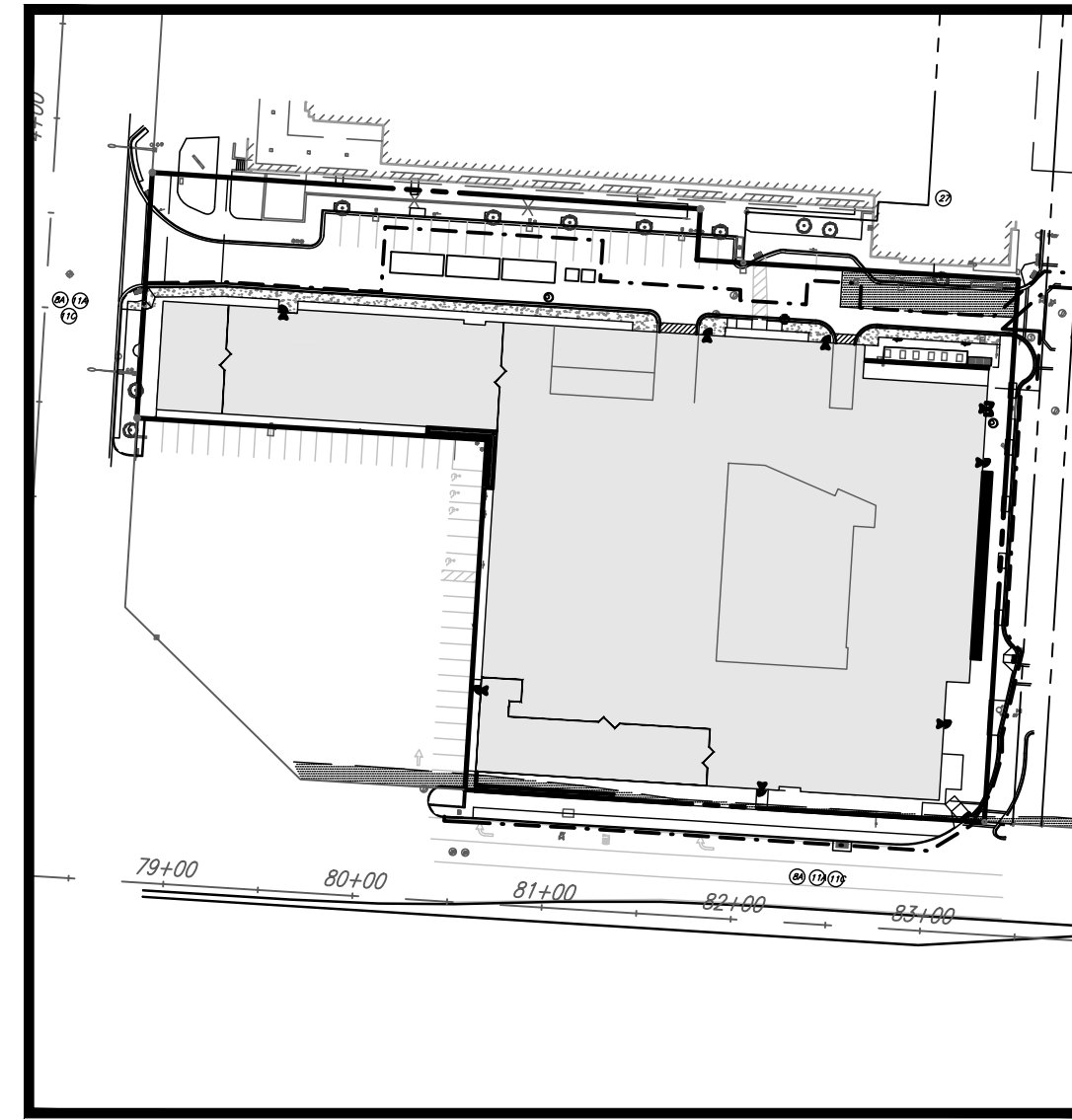
STANDARD ABBREVIATIONS FOR ENTIRE PLAN SET	
AC	ACRES
ADA	AMERICANS WITH DISABILITY ACT
ARCH	ARCHITECTURAL
BC	BOTTOM OF CURB
BF	BASEMENT FLOOR
BK	BLOCK
BL	BASELINE
BLDG	BUILDING
BM	BUILDING BENCHMARK
BRL	BUILDING RESTRICTION LINE
CF	CUBIC FEET
CL	CENTERLINE
CMP	CORRUGATED METAL PIPE
CONN	CONNECTION
CONC	CONCRETE
CPP	CORRUGATED PLASTIC PIPE
CY	CUBIC YARDS
DEC	DECORATIVE
DEP	DEPRESSED
DIP	DUCTILE IRON PIPE
DOM	DOMESTIC
ELEC	ELECTRIC
ELEV	ELEVATION
EP	EDGE OF PAVEMENT
ES	EDGE OF SHOULDER
EW	END WALL
EX	EXISTING
FES	FLARED END SECTION
FF	FINISHED FLOOR
FH	FIRE HYDRANT
FG	FINISHED GRADE
G	GRADE
GF	GARAGE FLOOR (AT DOOR)
GH	GRADE HIGHER SIDE OF WALL
GL	GRADE LOWER SIDE OF WALL
GRT	GRATE
GV	GATE VALVE
HDPE	HIGH DENSITY POLYETHYLENE PIPE
HP	HIGH POINT
HOR	HORIZONTAL
HW	HEADWALL
INT	INTERSECTION
INV	INVERT
LF	LINEAR FOOT
LOC	LIMITS OF CLEARING
LOD	LIMITS OF DISTURBANCE
LOS	LINE OF SIGHT
LP	LOW POINT
L/S	LANDSCAPE
MAX	MAXIMUM
MIN	MINIMUM
MH	MANHOLE
MJ	MECHANICAL JOINT
OC	ON CENTER
PA	POINT OF ANALYSIS
PC	POINT CURVATURE
PCCR	POINT OF COMPOUND CURVATURE, CURB RETURN
PI	POINT OF INTERSECTION
POG	POINT OF GRADE
PROP	PROPOSED
PT	POINT OF TANGENCY
PTCR	POINT OF TANGENCY, CURB RETURN
PVC	POLYVINYL CHLORIDE PIPE
PVI	POINT OF VERTICAL INTERSECTION
PVT	POINT OF VERTICAL TANGENCY
R	RADIUS
RCP	REINFORCED CONCRETE PIPE
RET WALL	RETAINING WALL
RW	RIGHT OF WAY
S	SLOPE
SAN	SANITARY SEWER
SF	SQUARE FEET
STA	STATION
STM	STORM
SW	SIDEWALK
TBR	TO BE REMOVED
TBRL	TO BE RELOCATED
TC	TOP OF CURB
TELE	TELEPHONE
TPF	TREE PROTECTION FENCE
TW	TOP OF WALL
TYP	TYPICAL
UG	UNDERGROUND
UP	UTILITY POLE
W	WIDE
WL	WATER LINE
WM	WATER METER
±	PLUS OR MINUS
°	DEGREE
Ø	DIAMETER
#	NUMBER

DETAILED SITE PLAN DSP - 21006

FOR

TDC PARCEL N INVESTOR, LLC

LOCATION OF SITE
6400 AMERICA BLVD
PRINCE GEORGE'S COUNTY
HYATTSVILLE, MARYLAND
TM: 42, GRID: A2



OWNER
BE UTC PARCEL N LLC
KEVIN BERMAN BERMAN ENT
5410 EDSON LANE SUITE 220
ROCKVILLE, MD 20852

DEVELOPER
TDC PARCEL N INVESTORS, LLC
NIHAR SHAH
1850 M STREET NW, SUITE 820
WASHINGTON, DC 20036
202-754-2145

PREPARED BY

BOHLER

CONTACT: NICHOLAS B. SPEACH, P.E.



LOCATION MAP
SCALE: 1" = 2000'

PLAN REFERENCES AND CONTACTS	
<p>REFERENCES</p> <ul style="list-style-type: none"> BOUNDARY & TOPOGRAPHIC SURVEY: CONTROL POINT ASSOCIATES, INC. "ALTANSPS LAND TITLE SURVEY" 6400 AMERICA BOULEVARD ELECTION DISTRICT NO. 17, PARCEL 17-3668724 CITY OF HYATTSVILLE PRINCE GEORGE'S COUNTY STATE OF MARYLAND DATED: 3/27/2020 FILE NO. 05-1990318 ELEVATIONS: MD STATE PLANE (NAD 83) GEOTECHNICAL REPORT: GETTECHNICAL SOLUTIONS, INC. "SUBSURFACE INVESTIGATION AND GEOTECHNICAL ENGINEERING REPORT UNIVERSITY TOWN CENTER - PARCEL N 6400 AMERICA BOULEVARD PRINCE GEORGE'S COUNTY, MD" GAS PROJECT NO. GP-202287 DATED: 01/29/2021 SITE DEVELOPMENT CONCEPT PLAN: BOHLER "SITE DEVELOPMENT CONCEPT PLAN SDCP# 2802-2021" DATED: 02/27/2021 APPROVED: 03/30/2021 PRELIMINARY PLAN: BOHLER "PRELIMINARY PLAN PPS-421006" DATED: 03/30/2021 APPROVED: TBD 	

THE ABOVE REFERENCED DOCUMENTS ARE INCORPORATED BY REFERENCE AS PART OF THESE PLANS, HOWEVER, BOHLER DOES NOT CERTIFY THE ACCURACY OF THE WORK REFERENCED OR DERIVED FROM THESE DOCUMENTS, BY OTHERS.

SHEET INDEX	
SHEET TITLE	SHEET NUMBER
COVER SHEET	DSP-1
PLAN APPROVALS SHEET	DSP-2
EXISTING CONDITIONS/DEMOLITION PLAN	DSP-3
SITE PLAN	DSP-4
GRADING / STORMDRAIN AND PAVING PLAN	DSP-5
PRE DEVELOPMENT DRAINAGE AREA MAPS	DSP-6
POST DEVELOPMENT DRAINAGE AREA MAP	DSP-7
SITE DETAILS	DSP-8
LANDSCAPE PLAN - STREETSCAPE	L0.01
LANDSCAPE PLANS - AMENITY SPACES	L0.02
HARDSCAPE DETAILS	L0.10
PLANTING DETAILS	L0.20
PLANTING NOTES + SCHEDULES	L0.21

	REQUIRED	PROPOSED
VEHICLE PARKING	NONE REQUIRED	272 SPACES
BICYCLE PARKING (10 SPACES FOR FIRST 100 PARKING SPACES + 10 SPACES FOR EVERY ADDITIONAL 100 PARKING SPACES)	30	61
MINIMUM PARKING SPACE DIMENSION (PERPENDICULAR PARKING)	9.5' X 19'	9.5' X 19' (ADA SPACES WITH 5'-10' WIDE EMBARK/DEBARK AREA)
DRIVE AISLE WIDTHS	22' FOR TWO-WAY TRAFFIC WITH PERPENDICULAR PARKING	22'
OFF-STREET LOADING SPACE(S) MINIMUM (ONE (1) SPACE FOR RETAIL AREA BETWEEN 2,000 TO 10,000 SQ. FT. OF GFA + ONE (1) FOR 100 TO 300 DWELLINGS + ONE (1) FOR EACH ADDITIONAL 200 DWELLINGS)	3	1
OFF STREET LOADING MINIMUM SIZE	12' X 33'	12' X 33'
PRINCIPAL BUILDING HEIGHT	16 STORY MAXIMUM	6 STORIES
BUILD-TO-LINE (BTL) - EAST-WEST HIGHWAY	25'-0"	25'-0"
BUILD-TO-LINE (BTL) - BELCREST ROAD	25'-0"	24'-0"
BUILD-TO-LINE (BTL) - AMERICA BLVD	20'-0"	15'-0"
BUILD-TO-LINE (BTL) - LIBERTY LANE	25'-0"	10'-0"

MAPPED SOIL TYPES		
Map Unit	Soil Description	Hydrologic Soil Group
UrB	URBAN LAND-RUSSETT-CHRISTIANA COMPLEX, 0 TO 5 PERCENT SLOPES	D

OBTAINED FROM: USDA, NRCS, WEB SOIL SURVEY (WSS) AVAILABLE ONLINE AT [HTTP://WEBSOILSURVEY.NRCS.USDA.GOV](http://websoilsurvey.nrcs.usda.gov)

BOHLER
SITE CIVIL AND CONSULTING ENGINEERING
PROGRAM MANAGEMENT
LANDSCAPE ARCHITECTURE
SUSTAINABLE DESIGN
PERMITTING SERVICES
TRANSPORTATION SERVICES

REVISIONS			
REV	DATE	COMMENT	DRAWN BY

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PROJECT No.: MB202020
DRAWN BY: AVB
CHECKED BY: NBS / AUL
DATE: 02/22/2021
CAD ID:

PROJECT:

DETAILED SITE PLAN
FOR
TDC PARCEL N INVESTOR, LLC

PROPOSED DEVELOPMENT
6400 AMERICA BLVD
PRINCE GEORGE'S COUNTY
HYATTSVILLE, MARYLAND
TM: 42, GRID: A2

BOHLER
16701 MELFORD BLVD., SUITE 310
BOWIE, MARYLAND 20715
Phone: (301) 809-4500
Fax: (301) 809-4501
MD@BohlerEng.com

NICHOLAS B. SPEACH
PROFESSIONAL ENGINEER
MAY 1988 - LICENSE NO. 8881
PROFESSIONAL CERTIFICATION
LANDSCAPE ARCHITECTURE
I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND. LICENSE NO. 42868, EXPIRATION DATE: 6/14/2023

SHEET TITLE:
COVER SHEET
SHEET NUMBER:
DSP-1
ORG. DATE - 02/22/2021

JUL 06, 2021 10:01 AM C:\NET\SHAH\MD\PROJECTS\2020\2020\DRAWINGS\PLAN SETS\DSP\MB202020 - DSP - 0 - LAYOUT DSP-1

SITE CIVIL AND CONSULTING ENGINEERING
 PROGRAM MANAGEMENT
 LANDSCAPE ARCHITECTURE
 SUSTAINABLE DESIGN
 PERMITTING SERVICES
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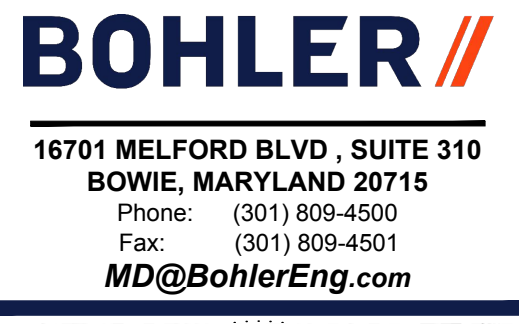
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PROJECT No.: MB202020
 DRAWN BY: AVB
 CHECKED BY: NBS / A.J.L.
 DATE: 02/22/2021
 CAD ID:

PROJECT:

DETAILED SITE PLAN
 FOR
 TDC PARCEL N
 INVESTOR, LLC

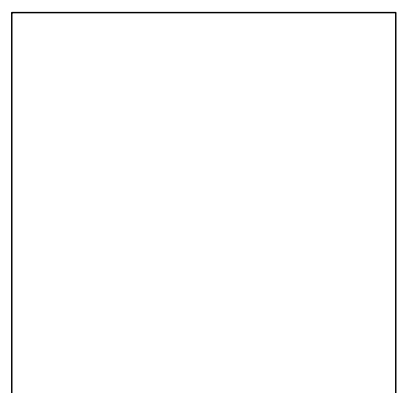
PROPOSED DEVELOPMENT
 6400 AMERICA BLVD
 PRINCE GEORGE'S COUNTY
 HYATTSVILLE, MARYLAND
 TM: 42, GRID: A2



SHEET TITLE:
PLAN APPROVALS SHEET

SHEET NUMBER:
DSP-2

ORG. DATE - 02/22/2021





**LIBERTY LANE
(PRIVATE ROAD)**
VARIABLE WIDTH RIGHT-OF-WAY

PARCEL 17-3566940
N/F LANDS OF
BE UTC CSA THEATRE, LLC.
D.B. 4201, PG. 475
ZONE: M-X-1
USE: COMMERCIAL
LIBER 34287 FOLIO 304
TM 0042 G.A2
1.7 AC

PARCEL 17-3666716
PARCEL "M"
(PER P.B. REP 205, PG. 43)
N/F LANDS OF
BE UTC CSA THEATRE, LLC.
D.B. 36078, PG. 1
ZONE: M-X-1
USE: COMMERCIAL
LIBER 36078 FOLIO 001
TM 0042 G.A2
.569 AC

PARCEL 17-3720745
N/F LANDS OF
ECHO UTC, LLC.
D.B. 35576, PG. 1
ZONE: M-X-1
USE: COMMERCIAL
LIBER 35576 FOLIO 001
TM 0042 G.A2
3.49 AC

PARCEL 17-3666724
PARCEL "N"
(PER P.B. REP 205, PG. 43)
N/F LANDS OF
BE UTC PARCEL N, LLC.
D.B. 36029, PG. 586
D.C. ZONE: M-U-1
AREA: 2.4216 AC
LIBER 36029 FOLIO 586
TM 0042 G.A2
2.42 AC

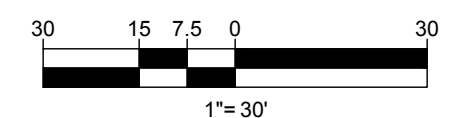
PROP. PARCEL
AREA = 2.43 AC
USE: MIXED-USE BUILDINGS

EAST-WEST HIGHWAY
(A.K.A. MD ROUTE 410)
(120' WIDE R.O.W.)

← TWO WAY TRAFFIC (ASPHALT ROADWAY) →

LEGEND

STEEP SLOPES 15 - 25%	[Symbol]
STEEP SLOPES >25%	[Symbol]



SURVEY NOTES:

1. PROPERTY KNOWN AS PARCEL 17-3666724 AS SHOWN ON THE OFFICIAL TAX ASSESSOR'S MAP OF PRINCE GEORGE'S COUNTY, MARYLAND, MAP NO. 042.
2. AREA = 105,484 S.F. OR 2.422 AC.
3. LOCATION OF UNDERGROUND UTILITIES ARE APPROXIMATE. LOCATIONS AND SIZES ARE BASED ON UTILITY MARK-OUTS, ABOVE GROUND STRUCTURES THAT WERE VISIBLE & ACCESSIBLE IN THE FIELD, AND THE MAPS AS LISTED IN THE REFERENCES AVAILABLE AT THE TIME OF THE SURVEY. AVAILABLE AS-BUILT PLANS AND UTILITY MARKOUT DOES NOT ENSURE MAPPING OF ALL UNDERGROUND UTILITIES AND STRUCTURES. BEFORE ANY EXCAVATION IS TO BEGIN, ALL UNDERGROUND UTILITIES SHOULD BE VERIFIED AS TO THEIR LOCATION, SIZE AND TYPE BY THE PROPER UTILITY COMPANIES. CONTROL POINT ASSOCIATES, INC. DOES NOT GUARANTEE THE UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA EITHER IN SERVICE OR ABANDONED.
4. THIS PLAN IS BASED ON INFORMATION PROVIDED BY A SURVEY PREPARED IN THE FIELD BY CONTROL POINT ASSOCIATES, INC. AND OTHER REFERENCE MATERIAL AS LISTED HEREON.
5. THIS SURVEY IS PREPARED WITH REFERENCE TO A TITLE REPORT PREPARED BY FIDELITY NATIONAL TITLE INSURANCE COMPANY, COMMITMENT NO. DC200478, WITH AN EFFECTIVE DATE OF FEBRUARY 10, 2020, WHERE THE FOLLOWING SURVEY RELATED EXCEPTIONS APPEAR IN SCHEDULE B, SECTION II:
 8. THE FOLLOWING MATTERS SHOWN AND/OR NOTED ON THE PLAT RECORDED IN PLAT BOOK VJ 163 AT PLAN NO. 19:
 - A 10' PUBLIC UTILITY EASEMENTS - SHOWN
 - B TERMS, CONDITIONS, EASEMENTS, MINIMUM BUILDING RESTRICTIONS LINES (BUT NOT SHOWN) CONTAINED IN THE OWNER'S DEDICATION AND THE NOTES - BLANKET IN NATURE
 - C INCORPORATION BY REFERENCE OF THE TERMS AND PROVISIONS OF LIBER 3703 AT FOLIO 748 - PUBLIC UTILITY EASEMENTS SHOWN
 9. THE FOLLOWING MATTERS SHOWN AND/OR NOTED ON THE PLAT RECORDED IN PLAT BOOK WWW 44 AT PLAN NO. 70:
 - A TERMS, CONDITIONS, MINIMUM BUILDING RESTRICTIONS LINES (BUT NOT SHOWN) CONTAINED IN THE OWNER'S DEDICATION - BLANKET IN NATURE
 10. THE FOLLOWING MATTERS SHOWN AND/OR NOTED ON THE PLAT RECORDED IN PLAT BOOK WWW 72 AT PLAN NO. 43:
 - A TERMS, CONDITIONS, MINIMUM BUILDING RESTRICTIONS LINES (BUT NOT SHOWN) CONTAINED IN THE OWNER'S DEDICATION - BLANKET IN NATURE
 11. THE FOLLOWING MATTERS SHOWN AND/OR NOTED ON THE PLAT RECORDED IN PLAT BOOK REP 205 AT PLAN NO. 43:
 - A 10' PUBLIC UTILITY EASEMENT - SHOWN
 - B TERMS, CONDITIONS, EASEMENTS, RESTRICTIONS, MINIMUM BUILDING RESTRICTIONS LINES (BUT NOT SHOWN) CONTAINED IN THE OWNER'S DEDICATION AND THE NOTES - BLANKET IN NATURE
 - C INCORPORATION BY REFERENCE OF THE TERMS AND PROVISIONS OF LIBER 3703 AT FOLIO 748 - PUBLIC UTILITY EASEMENTS SHOWN
 12. TERMS, CONDITIONS, RESTRICTIONS, AND EASEMENTS CONTAINED IN THE INSTRUMENT DATED FEBRUARY 19, 1920 AND RECORDED FEBRUARY 21, 1920 IN LIBER 141 AT FOLIO 395 - LOCATION CANNOT BE DETERMINED FROM RECORD DOCUMENT
 13. TERMS, CONDITIONS, RESTRICTIONS, AND EASEMENTS CONTAINED IN THE INSTRUMENT DATED APRIL 30, 1947 AND RECORDED JUNE 11, 1947 IN LIBER 940 AT FOLIO 181 - LOCATION CANNOT BE DETERMINED FROM RECORD DOCUMENT
 14. TERMS, CONDITIONS, RESTRICTIONS, AND EASEMENTS CONTAINED IN THE RIGHT OF WAY DATED AUGUST 11, 1950 AND RECORDED OCTOBER 23, 1950 IN LIBER 1287 AT FOLIO 42 - LOCATION CANNOT BE DETERMINED FROM RECORD DOCUMENT
 15. TERMS, CONDITIONS, RESTRICTIONS, AND EASEMENTS CONTAINED IN THE INSTRUMENT DATED NOVEMBER 14, 1950 AND RECORDED FEBRUARY 15, 1951 IN LIBER 1328 AT FOLIO 253 - LOCATION CANNOT BE DETERMINED FROM RECORD DOCUMENT
 16. TERMS, CONDITIONS, RESTRICTIONS, AND EASEMENTS CONTAINED IN THE RIGHT OF WAY DATED DECEMBER 11, 1951 AND RECORDED MARCH 4, 1952 IN LIBER 1472 AT FOLIO 92 - LOCATION CANNOT BE DETERMINED FROM RECORD DOCUMENT
 17. TERMS, CONDITIONS, RESTRICTIONS, AND EASEMENTS CONTAINED IN THE DEED DATED DECEMBER 14, 1954 AND RECORDED DECEMBER 23, 1954 IN LIBER 1807 AT FOLIO 141 AND THE PLATS RECORDED AS STATE ROADS COMMISSION OF MARYLAND PLAT NOS. 13528, 13529, 13530, 13531, 13532, 13533, 13534, 13535 AND 13537 - SHOWN
 18. TERMS, CONDITIONS, RESTRICTIONS AND EASEMENTS CONTAINED IN THE RIGHT OF WAY DATED FEBRUARY 1, 1963 AND RECORDED FEBRUARY 8, 1963 IN LIBER 2784 AT FOLIO 31 - NOT ON SUBJECT PROPERTY
 19. TERMS, COVENANTS, CONDITIONS, RESTRICTIONS AND PROVISIONS CONTAINED IN THE AGREEMENT DATED MAY 26, 1970 AND RECORDED JUNE 11, 1970 IN LIBER 3838 AT FOLIO 90 - BLANKET IN NATURE
 20. TERMS, CONDITIONS AND EASEMENTS CONTAINED IN THE DECLARATION OF EASEMENT DATED NOVEMBER 6, 1974 AND RECORDED NOVEMBER 6, 1974 IN LIBER 4432 AT FOLIO 169; AS AFFECTED BY THE DECLARATION OF EASEMENT AND COVENANT DATED DECEMBER 19, 2002 AND RECORDED JANUARY 13, 2003 IN LIBER 16693 AT FOLIO 721 - NOT ON SUBJECT PROPERTY
 21. TERMS, COVENANTS, CONDITIONS, RESTRICTIONS, PROVISIONS COSTS AND REIMBURSEMENTS AND EASEMENTS CONTAINED IN THE DECLARATION OF EASEMENT AND COVENANT DATED DECEMBER 19, 2002 AND RECORDED JANUARY 13, 2003 IN LIBER 16693 AT FOLIO 721 - NOT ON SUBJECT PROPERTY
 22. TERMS, COVENANTS, CONDITIONS, PROVISIONS FOR COSTS AND REIMBURSEMENTS, AND EASEMENTS CONTAINED IN THE DECLARATION OF COVENANTS FOR STORM AND SURFACE WATER FACILITY AND SYSTEM MAINTENANCE DATED APRIL 26, 2004 AND RECORDED MAY 10, 2004 IN LIBER 19476 AT FOLIO 540 - NOT ON SUBJECT PROPERTY
 23. TERMS, CONDITIONS, RESTRICTIONS AND EASEMENTS CONTAINED IN THE RIGHT OF WAY DATED JUNE 3, 2004 AND RECORDED JULY 28, 2004 IN LIBER 20046 AT FOLIO 148 - NOT ON SUBJECT PROPERTY
 24. TERMS, CONDITIONS, AND PROVISIONS CONTAINED IN THE NOTICE OF CREATION OF CITY OF HYATTSVILLE UNIVERSITY TOWN CENTER SPECIAL TAXING DISTRICT AND IMPOSITION OF SPECIAL TAX DATED AUGUST 24, 2004 AND RECORDED AUGUST 26, 2004 IN LIBER 20209 AT FOLIO 228 - NOT SURVEY RELATED
 25. TERMS, COVENANTS, CONDITIONS, PROVISIONS FOR COSTS AND REIMBURSEMENTS, AND EASEMENTS CONTAINED IN THE DECLARATION OF COVENANTS FOR STORM AND SURFACE WATER FACILITY AND SYSTEM MAINTENANCE DATED APRIL 8, 2005 AND RECORDED APRIL 19, 2005 IN LIBER 21838 AT FOLIO 467 - BLANKET IN NATURE
 26. TERMS, COVENANTS, CONDITIONS, RESTRICTIONS, PROVISIONS FOR ASSESSMENT AND LIENS, AND EASEMENTS CONTAINED IN THE DECLARATION OF COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS FOR UNIVERSITY TOWN CENTER DATED OCTOBER 31, 2006 AND RECORDED NOVEMBER 13, 2006 IN LIBER 26401 AT FOLIO 132; AS AFFECTED BY THE FIRST AMENDMENT TO DECLARATION OF COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS FOR UNIVERSITY TOWN CENTER DATED MARCH 13, 2015 AND RECORDED MARCH 19, 2015 IN LIBER 36798 AT FOLIO 640 - BLANKET IN NATURE
 27. TERMS, CONDITIONS, RESTRICTIONS, AND EASEMENTS CONTAINED IN THE RIGHT OF WAY DATED APRIL 18, 2007 AND RECORDED AUGUST 13, 2007 IN LIBER 28391 AT FOLIO 54 - SHOWN
 28. TERMS, COVENANTS, CONDITIONS, PROVISIONS FOR COSTS AND REIMBURSEMENTS, AND EASEMENTS CONTAINED IN THE DECLARATION OF COVENANTS FOR STORM AND SURFACE WATER FACILITY AND SYSTEM MAINTENANCE DATED MARCH 19, 2009 AND RECORDED MARCH 24, 2009 IN LIBER 30464 AT FOLIO 412 - BLANKET IN NATURE
 29. TERMS, COVENANTS, CONDITIONS, RESTRICTIONS, AND PROVISIONS CONTAINED IN THE AMENDED AND RESTATE DEVELOPMENT COVENANT DECLARATION DATED OCTOBER 18, 2012 AND RECORDED DECEMBER 13, 2012 IN LIBER 34197 AT FOLIO 57, WHICH AMENDS AND RESTATES THE DEVELOPMENT COVENANT AND DECLARATION DATED AS OF OCTOBER 31, 2006 AND RECORDED NOVEMBER 13, 2006 IN LIBER 26401 AT FOLIO 116 - BLANKET IN NATURE
6. BY GRAPHIC PLOTTING ONLY PROPERTY IS LOCATED IN FLOOD HAZARD ZONE X (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN) PER REF. #2.
7. THE EXISTENCE OF UNDERGROUND STORAGE TANKS, IF ANY, WAS NOT KNOWN AT THE TIME OF THE FIELD SURVEY.
8. ELEVATIONS REFER TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88, BASED ON GPS OBSERVATIONS UTILIZING THE KEYSTONE VRS NETWORK (KEYNETGPS).
TEMPORARY BENCH MARKS SET:
TBM-A: MAG NAIL SET ELEVATION = 123.23
TBM-B: MAG NAIL SET ELEVATION = 103.61
PRIOR TO CONSTRUCTION IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THAT THE BENCHMARKS ILLUSTRATED ON THIS SKETCH HAVE NOT BEEN DISTURBED AND THEIR ELEVATIONS HAVE BEEN CONFIRMED. ANY CONFLICTS MUST BE REPORTED PRIOR TO CONSTRUCTION.
9. THE OFFSETS SHOWN ARE NOT TO BE USED FOR THE CONSTRUCTION OF ANY STRUCTURE, FENCE, PERMANENT ADDITION, ETC.

BOHLER
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REVISIONS

REV	DATE	COMMENT	DRAWN BY	CHECKED BY

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DRAWN BY: AVB
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DATE: 02/22/2021
CAD ID: DRAWING1

DETAILED SITE PLAN
FOR
TDC PARCEL N INVESTOR, LLC
PROPOSED DEVELOPMENT
6400 AMERICA BLVD
PRINCE GEORGE'S COUNTY
HYATTSVILLE, MARYLAND
TM. 42, GRID. A2

BOHLER
16701 MELFORD BLVD., SUITE 310
BOWIE, MARYLAND 20715
Phone: (301) 809-4500
Fax: (301) 809-4501
MD@BohlerEng.com

NICHOLAS B. SPEACH
PROFESSIONAL ENGINEER
LICENSED UNDER THE STATE OF MARYLAND
PROFESSIONAL CERTIFICATION NUMBER: 17456
I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND.
LICENSE NO. 42888, EXPIRATION DATE: 6/14/2023

SHEET TITLE:
EXISTING CONDITIONS/DEMOLITION PLAN
SHEET NUMBER:
DSP-3
ORG. DATE - 02/22/2021

JUL 06, 2021 11:00:18 AM C:\NETSHARED\MOD-PROJECTS\2020\2020\DRAWINGS\PLAN SETS\DISP\MB202020 - DISP - 0 - LAYOUT - DSP-3



**LIBERTY LANE
(PRIVATE ROAD)**
VARIABLE WIDTH RIGHT-OF-WAY

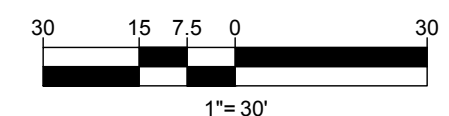
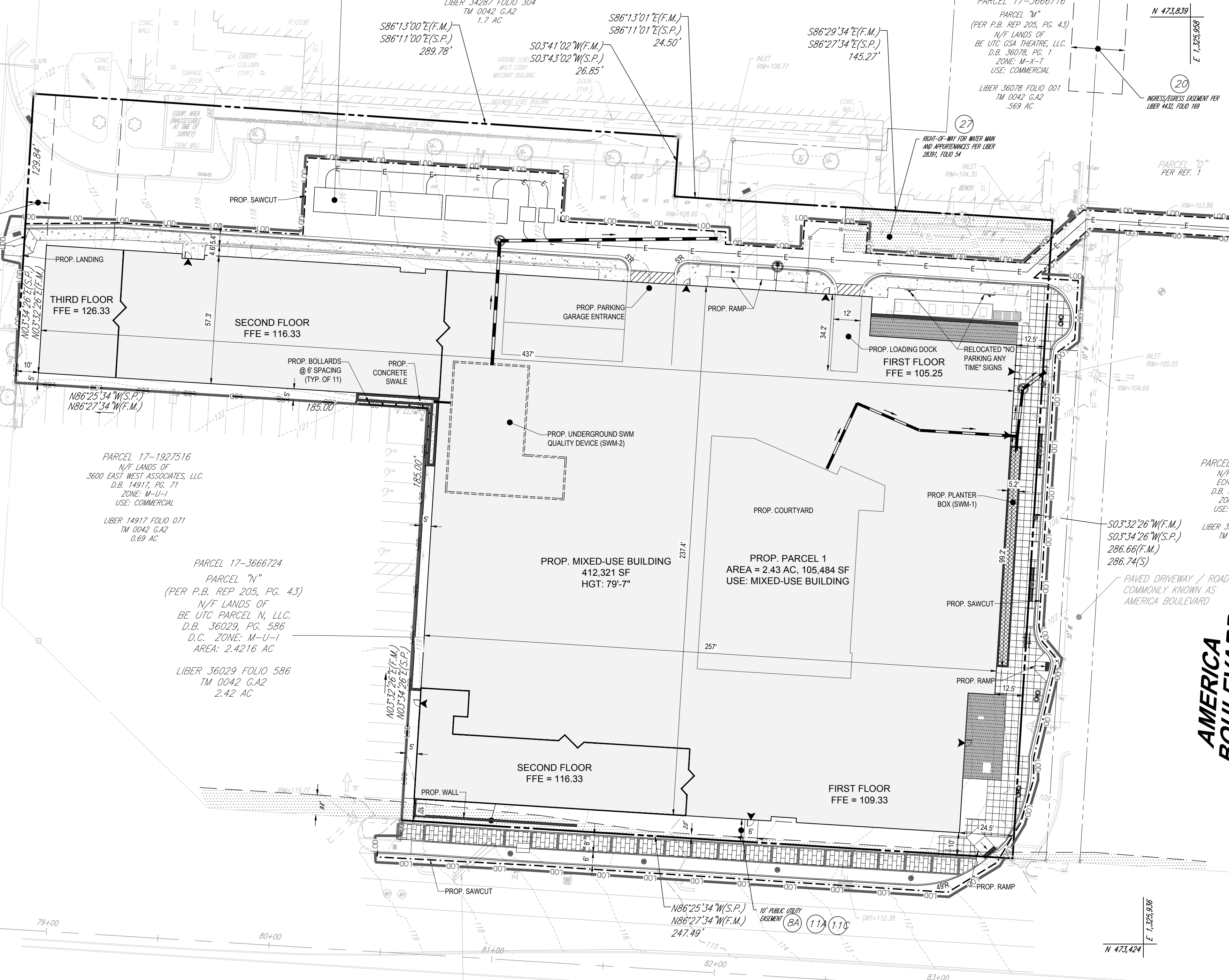
BELCREST ROAD

**AMERICA BOULEVARD
(PRIVATE ROAD)**
VARIABLE WIDTH RIGHT-OF-WAY

EAST-WEST HIGHWAY
(A.K.A. MD ROUTE 410)
(120' WIDE R.O.W.)
TWO WAY TRAFFIC
(ASPHALT ROADWAY)

HATCH LEGEND

CONCRETE SIDEWALK	
CONCRETE PAVER	
BRICK PAVER	



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SITE CIVIL AND CONSULTING ENGINEERING
PROGRAM MANAGEMENT
LANDSCAPE ARCHITECTURE
SUSTAINABLE DESIGN
PERMITTING SERVICES
TRANSPORTATION SERVICES

REVISIONS

REV	DATE	COMMENT	DRAWN BY	CHECKED BY

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THIS DRAWING IS INTENDED FOR MUNICIPAL AND/OR AGENCY REVIEW AND APPROVAL. IT IS NOT INTENDED AS A CONSTRUCTION DOCUMENT UNLESS INDICATED OTHERWISE.

PROJECT No.: MB202020
DRAWN BY: AVB
CHECKED BY: NBS / AJL
DATE: 02/22/2021
CAD ID: DRAWING1

PROJECT:
DETAILED SITE PLAN
FOR
TDC PARCEL N INVESTOR, LLC
PROPOSED DEVELOPMENT
6400 AMERICA BLVD
PRINCE GEORGE'S COUNTY
HYATTSVILLE, MARYLAND
TM: 42, GRID: A2

BOHLER
16701 MELFORD BLVD., SUITE 310
BOWIE, MARYLAND 20715
Phone: (301) 809-4500
Fax: (301) 809-4501
MD@BohlerEng.com

NICHOLAS D. SPEACH
PROFESSIONAL ENGINEER
MAY 1980 LICENSE NO. 5881
PROFESSIONAL CERTIFICATION
I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND. LICENSE NO. 4288, EXPIRATION DATE: 6/14/2023

SHEET TITLE:
SITE PLAN
SHEET NUMBER:
DSP-4
ORG. DATE - 02/22/2021



**LIBERTY LANE
(PRIVATE ROAD)**
VARIABLE WIDTH RIGHT-OF-WAY

PARCEL 17-3566940
N/F LANDS OF
BE UTC CSA THEATRE, LLC.
D.B. 4201, PG. 475
ZONE: M-X-1
USE: COMMERCIAL
LIBER 34287 FOLIO 304
TM 0042 G.A2
1.7 AC

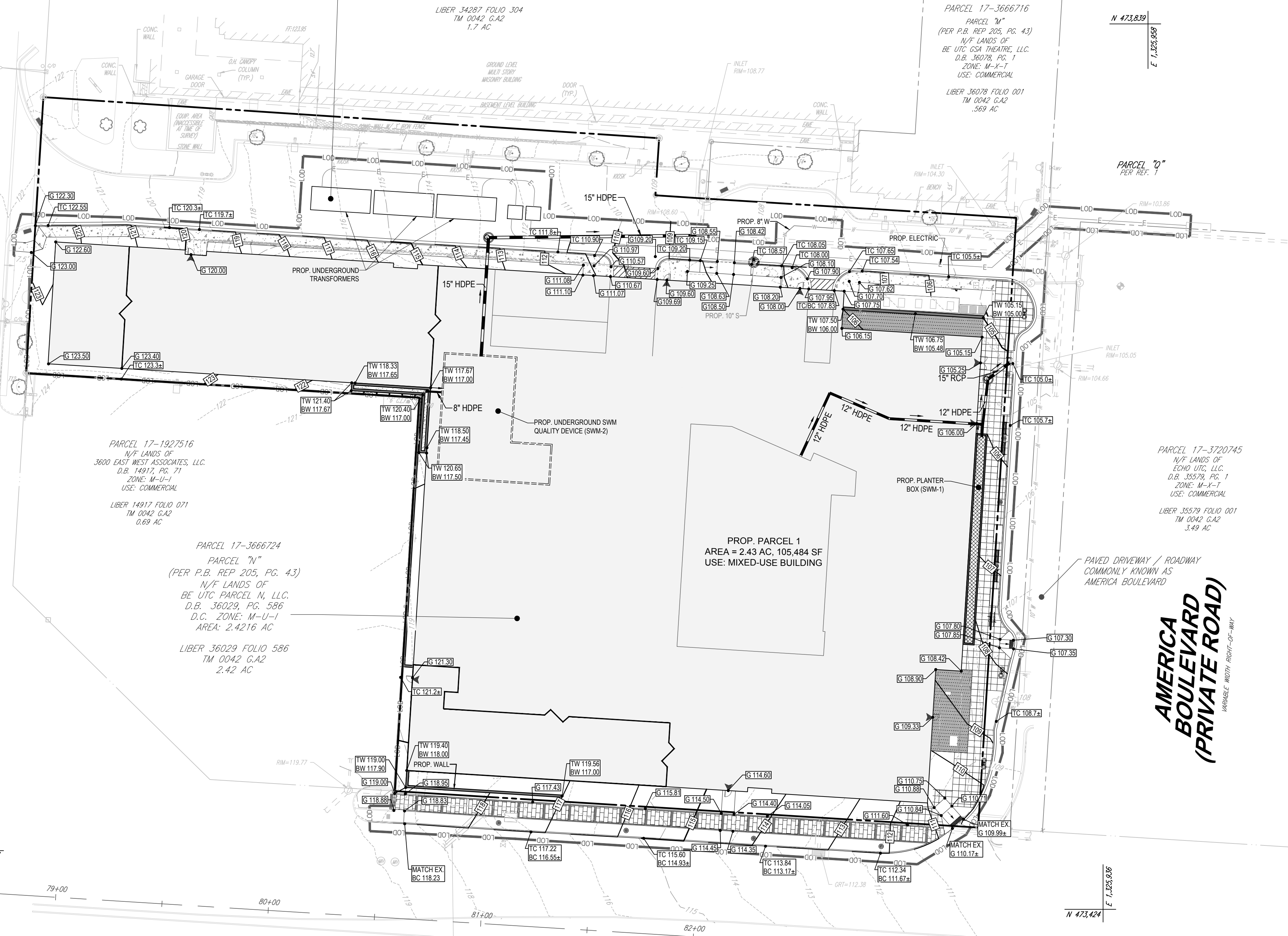
PARCEL 17-3666716
PARCEL "M"
(PER P.B. REP 205, PG. 43)
N/F LANDS OF
BE UTC CSA THEATRE, LLC.
D.B. 36078, PG. 1
ZONE: M-X-1
USE: COMMERCIAL
LIBER 36078 FOLIO 001
TM 0042 G.A2
.569 AC

N 473,819
E 1,225,609

N 473,819
E 1,225,609

BELCREST ROAD
(104' WIDE R.O.W.)
TWO WAY TRAFFIC
(ASPHALT ROADWAY)

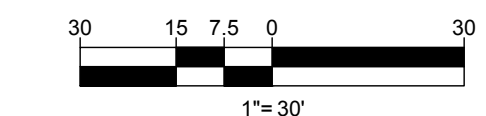
4+00
E 1,225,609
N 473,819
3+00
2+00
1+00
E 1,225,602
N 473,457
78+00
79+00
80+00
81+00
82+00
83+00
84+00



PROP. PARCEL 1
AREA = 2.43 AC, 105,484 SF
USE: MIXED-USE BUILDING

**AMERICA BOULEVARD
(PRIVATE ROAD)**
VARIABLE WIDTH RIGHT-OF-WAY

EAST-WEST HIGHWAY
(A.K.A. MD ROUTE 410)
(120' WIDE R.O.W.)
TWO WAY TRAFFIC
(ASPHALT ROADWAY)



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PROJECT NO.: MB202020
DRAWN BY: AVB
CHECKED BY: NBS / AJL
DATE: 02/22/2021
CAD ID: DRAWING1

DETAILED SITE PLAN
FOR
TDC PARCEL N INVESTOR, LLC
PROPOSED DEVELOPMENT
6400 AMERICA BLVD
PRINCE GEORGE'S COUNTY
HYATTSVILLE, MARYLAND
TM: 42, GRID: A2

BOHLER
16701 MELFORD BLVD., SUITE 310
BOWIE, MARYLAND 20715
Phone: (301) 809-4500
Fax: (301) 809-4501
MD@BohlerEng.com

NICHOLAS D. SPEACH
PROFESSIONAL ENGINEER
LICENSED UNDER THE STATE OF MARYLAND
EXPIRES 07/01/21
I, NICHOLAS D. SPEACH, PROFESSIONAL ENGINEER, HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND. LICENSE NO. 42083, EXPIRATION DATE: 6/14/2023

SHEET TITLE
**GRADING/
STORMDRAIN
AND PAVING
PLAN**
SHEET NUMBER
DSP-5
ORG. DATE - 02/22/2021



**LIBERTY LANE
(PRIVATE ROAD)**

BELCREST ROAD

**AMERICA BOULEVARD
(PRIVATE ROAD)**

EAST-WEST HIGHWAY

DA-1

DA-2

POI-1

DA-3

DA-4

DA-5

DA-6

POI-2

PROP PARCEL
AREA = 2.43 AC. 107,484 SF
USE: MIXED-USE BUILDING

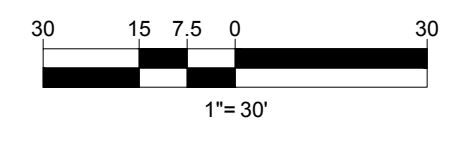
PROP WALL

LEGEND

- LIMIT OF DISTURBANCE
- PROPOSED DRAINAGE DIVIDE
- DRAINAGE AREA LABEL
- POINT OF INTEREST LABEL
- STEEP SLOPES 15 - 25%
- STEEP SLOPES >25%

PRE-DEVELOPMENT DRAINAGE AREA TABLE			
DRAINAGE AREA	AREA (AC.)	IMPERVIOUS AREA (AC.)	C VALUE
DA-1	0.35	0.33	0.87
DA-2	1.20	0.86	0.73
DA-3	0.25	0.20	0.78
DA-4	0.19	0.19	0.90
DA-5	0.72	0.54	0.75
DA-6	0.22	0.21	0.87

10-YR PRE-DEVELOPMENT LOI ANALYSIS	
LINEPOINT OF INTEREST	PRE DEVELOPMENT FLOW
POI-1	14.26 CFS
POI-2	1.18 CFS



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DRAWN BY: AVB
CHECKED BY: NBS / AJL
DATE: 02/22/2021
CAD ID: DRAWING1

DETAILED SITE PLAN
FOR
TDC PARCEL N INVESTOR, LLC

PROPOSED DEVELOPMENT
6400 AMERICA BLVD
PRINCE GEORGE'S COUNTY
HYATTSVILLE, MARYLAND
TM: 42, GRID: A2

BOHLER
16701 MELFORD BLVD., SUITE 310
BOWIE, MARYLAND 20715
Phone: (301) 809-4500
Fax: (301) 809-4501
MD@BohlerEng.com

NICHOLAS B. SPEACH
PROFESSIONAL ENGINEER
MARYLAND LICENSE NO. 42083
EXPIRES 07/01/21
I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND.
LICENSE NO. 42083, EXPIRATION DATE: 6/14/2023

SHEET TITLE:
PRE DEVELOPMENT DRAINAGE AREA MAP
SHEET NUMBER:
DSP-6
ORG. DATE - 02/22/2021

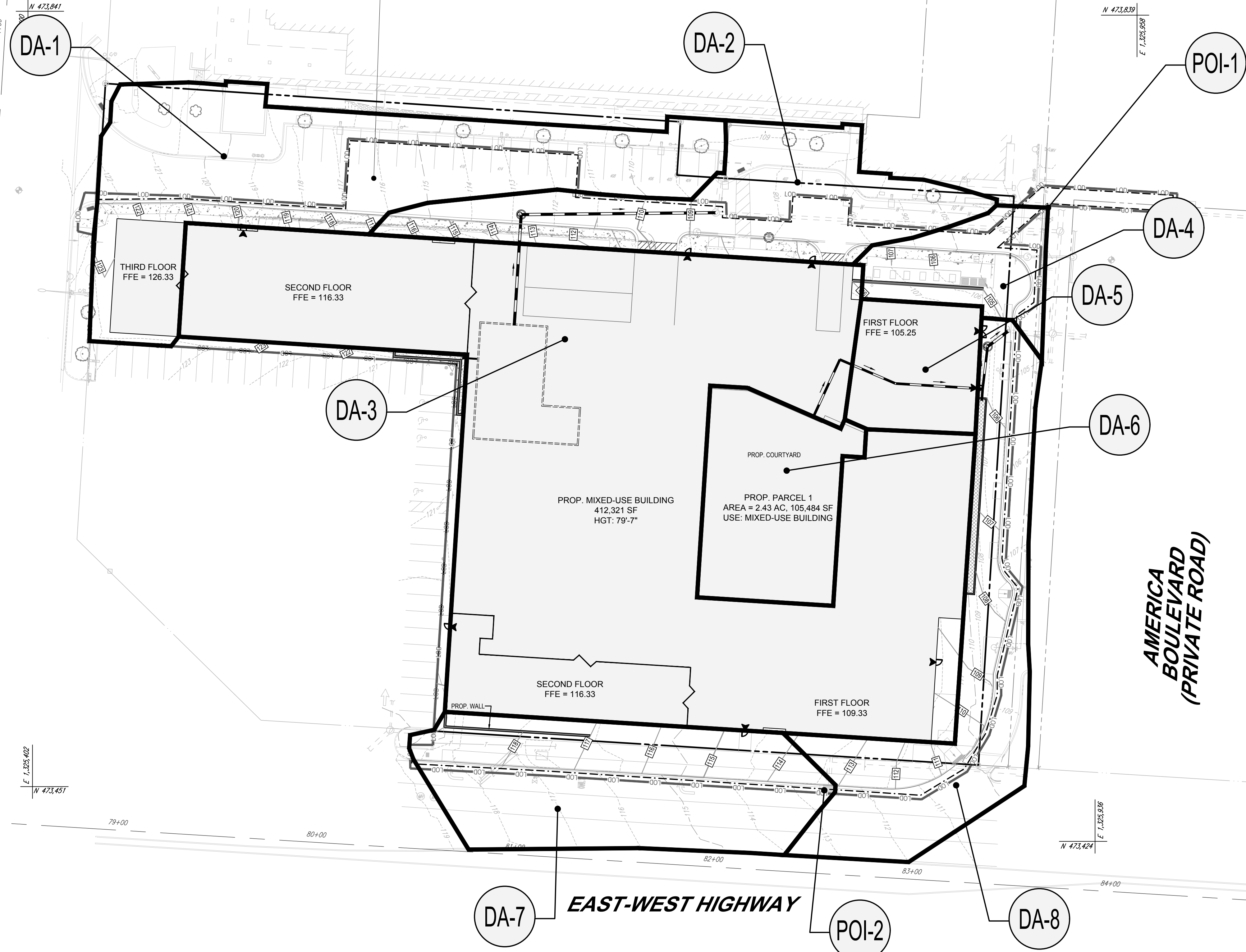


**LIBERTY LANE
(PRIVATE ROAD)**

BELCREST ROAD

**AMERICA
BOULEVARD
(PRIVATE ROAD)**

EAST-WEST HIGHWAY



LEGEND

- LIMIT OF DISTURBANCE
- PROPOSED DRAINAGE DIVIDE
- DRAINAGE AREA LABEL DA-X
- POINT OF INTEREST LABEL POI-X

PRE-DEVELOPMENT DRAINAGE AREA TABLE

DRAINAGE AREA	AREA (AC.)	IMPERVIOUS AREA (AC.)	C VALUE
DA-1	0.43	0.33	0.76
DA-2	0.24	0.21	0.83
DA-3	1.31	1.31	0.90
DA-4	0.09	0.06	0.70
DA-5	0.09	0.09	0.90
DA-6	0.16	0.16	0.90
DA-7	0.28	0.21	0.75
DA-8	0.28	0.20	0.73

10-YR POST-DEVELOPMENT LOI ANALYSIS

LINEPOINT OF INTEREST	POST DEVELOPMENT FLOW
POI-1	13.86 CFS
POI-2	1.46 CFS

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PROJECT No.: MB202020
 DRAWN BY: AVB
 CHECKED BY: NBS / AUL
 DATE: 02/22/2021
 CAD ID: DRAWING1

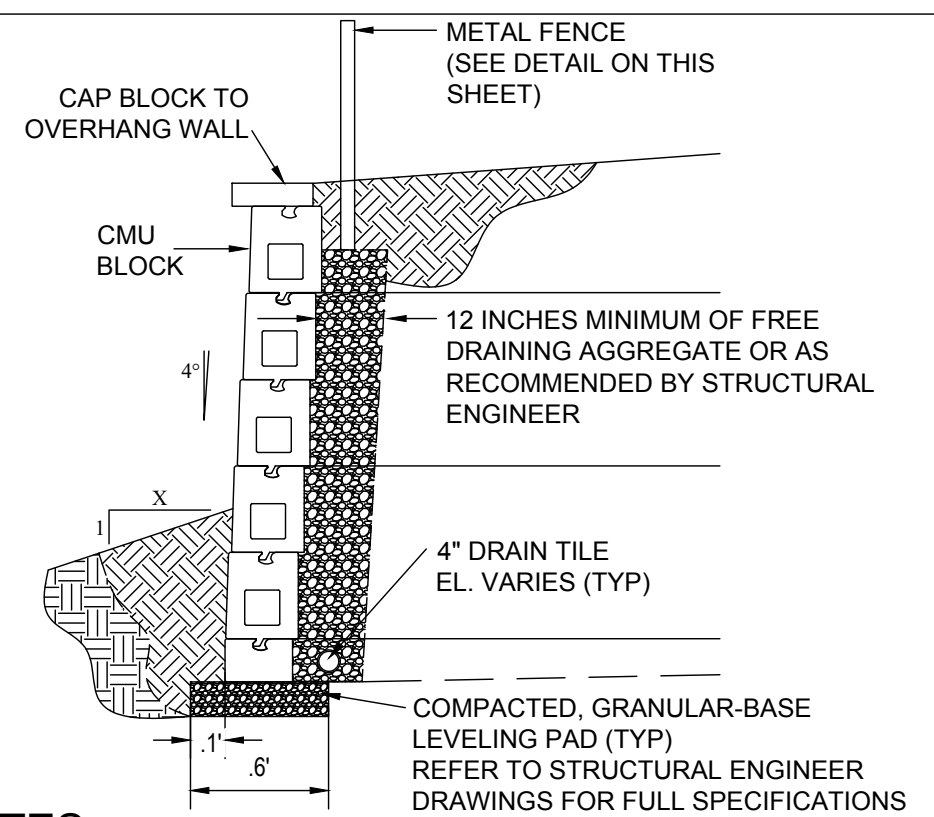
DETAILED SITE PLAN
 FOR
 TDC PARCEL N
 INVESTOR, LLC
 PROPOSED DEVELOPMENT
 6400 AMERICA BLVD
 PRINCE GEORGE'S COUNTY
 HYATTSVILLE, MARYLAND
 TM: 42, GRID: A2

BOHLER
 16701 MELFORD BLVD., SUITE 310
 BOWIE, MARYLAND 20715
 Phone: (301) 809-4500
 Fax: (301) 809-4501
 MD@BohlerEng.com

NICHOLAS B. SPEACH
 PROFESSIONAL ENGINEER
 LICENSE NO. 42863, EXPIRATION DATE: 6/14/2023

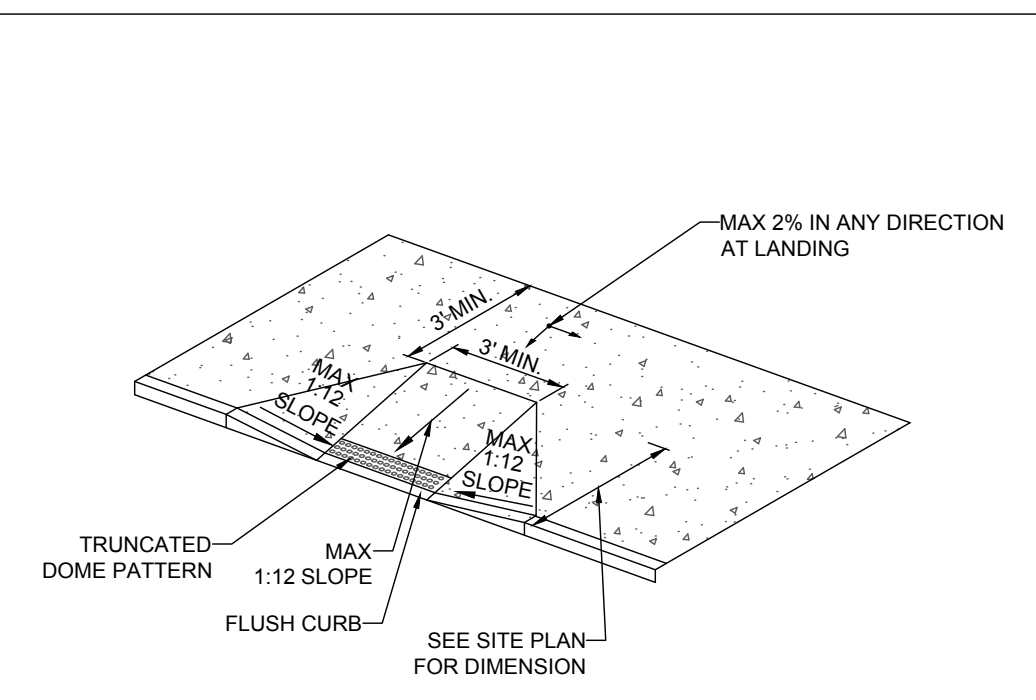
SHEET TITLE: **POST DEVELOPMENT DRAINAGE AREA MAP**
 SHEET NUMBER: **DSP-7**
 ORG. DATE - 02/22/2021

JUL 06, 2021 10:08 AM BOHLER ENG NET\SHAMES.MD\PROJECTS\2020\2021\DRAWINGS\PLAN SETS\DISP\MB202020 - HDP - 0 - LAYOUT.DWG 7



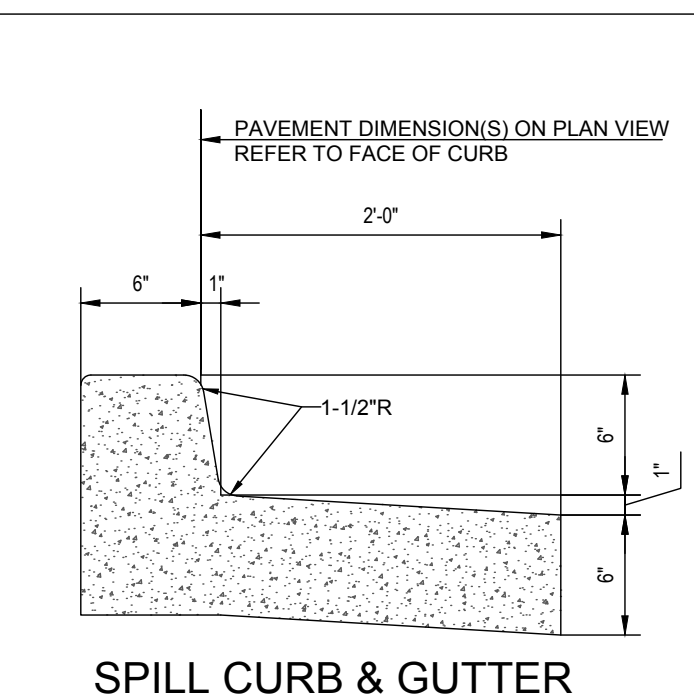
NOTES:
REFER TO RETAINING WALL DESIGNER AND STRUCTURAL ENGINEER FOR FINAL DESIGN PLANS. THE CONSULTANT DESIGNING THE RETAINING WALL SHALL USE THIS DETAIL AS A GUIDELINE

RETAINING WALL DETAIL
NOT TO SCALE



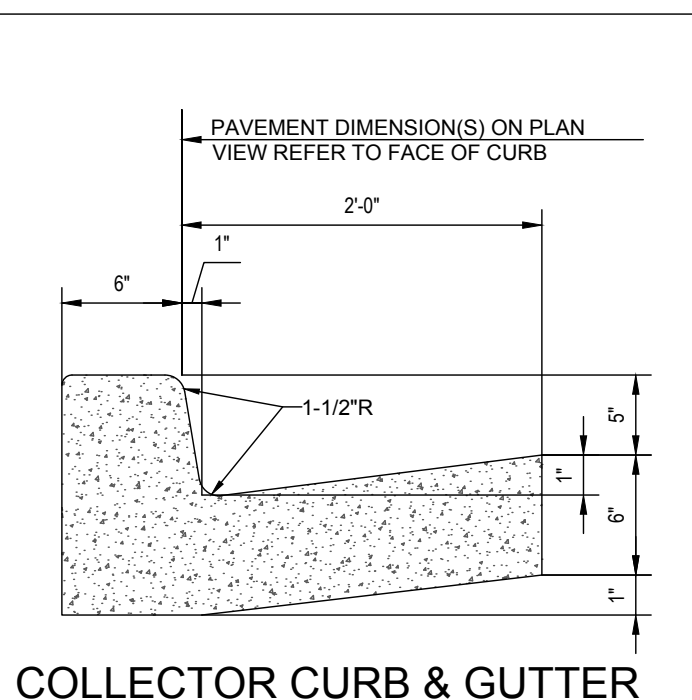
NOTE:
1. ADA ACCESSIBLE RAMP CONSTRUCTION SHALL CONFORM TO CURRENT ADA ACCESSIBLE GUIDELINES.

ADA ACCESSIBLE RAMP DETAIL
NOT TO SCALE



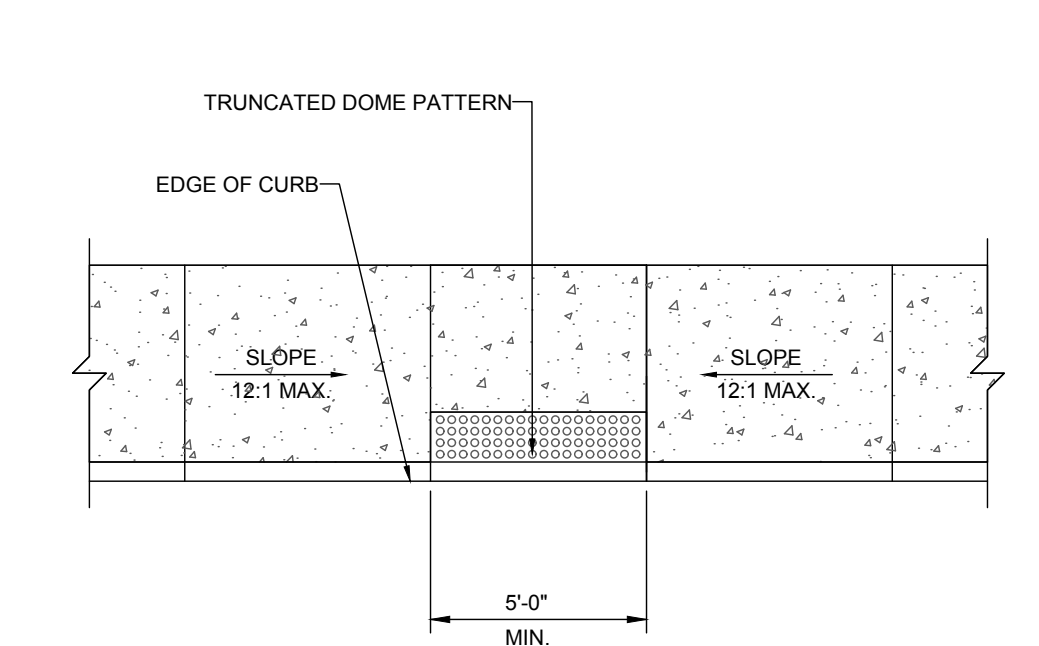
NOTES:
1. CONCRETE FOR CURBING SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3,000 P.S.I. @ 28 DAYS.
2. CONSTRUCTION STAKING FOR CURB INSTALLATION SHALL BE REFERENCED (CUT OR FILL) TO THE TOP OF CURB.
3. AT CONTRACTOR'S OPTION, THE GUTTER THICKNESS MAY BE INCREASED AT THE EDGE OF PAVEMENT TO MAKE BOTTOM OF GUTTER CONTIGUOUS WITH BOTTOM OF ASPHALT PAVEMENT.
4. CONTRACTION JOINTS SHALL BE PLACED @ 10'-0\"/>

CONCRETE CURB & GUTTER DETAIL
NOT TO SCALE



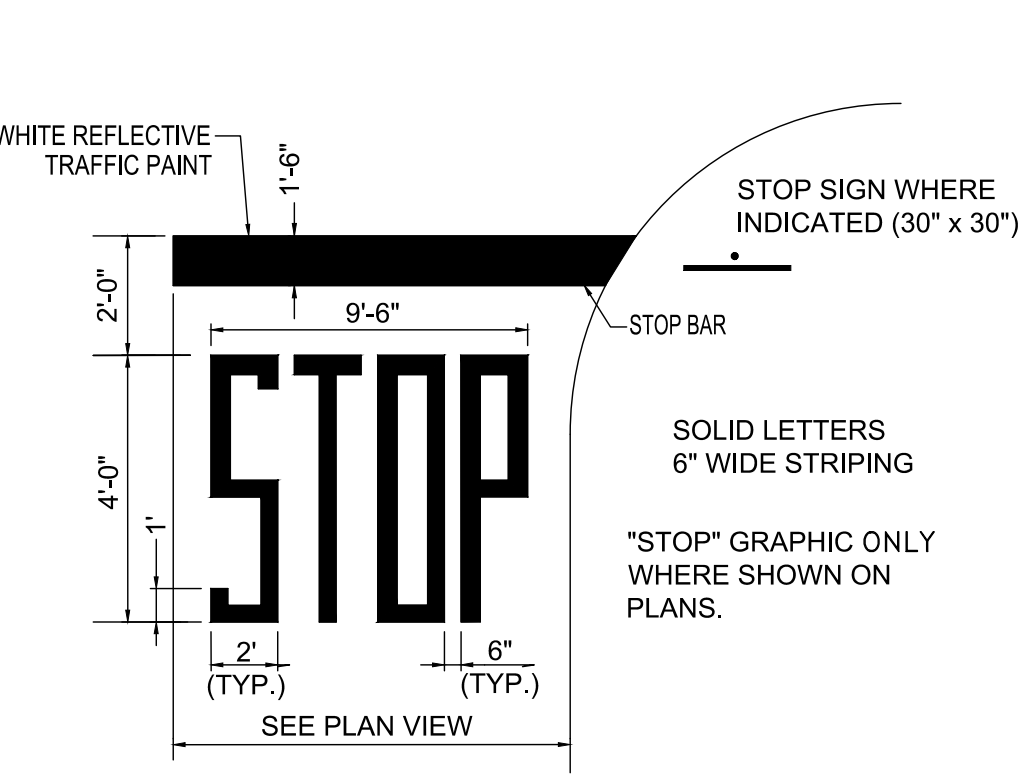
NOTES:
1. CONCRETE FOR CURBING SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3,000 P.S.I. @ 28 DAYS.
2. CONSTRUCTION STAKING FOR CURB INSTALLATION SHALL BE REFERENCED (CUT OR FILL) TO THE TOP OF CURB.
3. AT CONTRACTOR'S OPTION, THE GUTTER THICKNESS MAY BE INCREASED AT THE EDGE OF PAVEMENT TO MAKE BOTTOM OF GUTTER CONTIGUOUS WITH BOTTOM OF ASPHALT PAVEMENT.
4. CONTRACTION JOINTS SHALL BE PLACED @ 10'-0\"/>

COLLECTOR CURB & GUTTER DETAIL
NOT TO SCALE

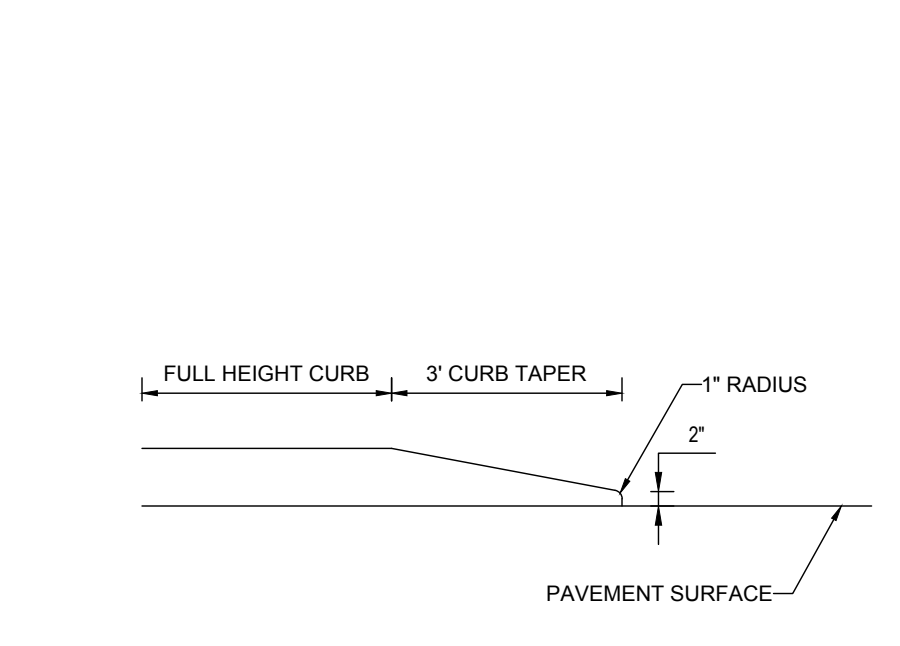


NOTE:
ADA ACCESSIBLE RAMP CONSTRUCTION SHALL CONFORM TO CURRENT ADA ACCESSIBLE GUIDELINES.

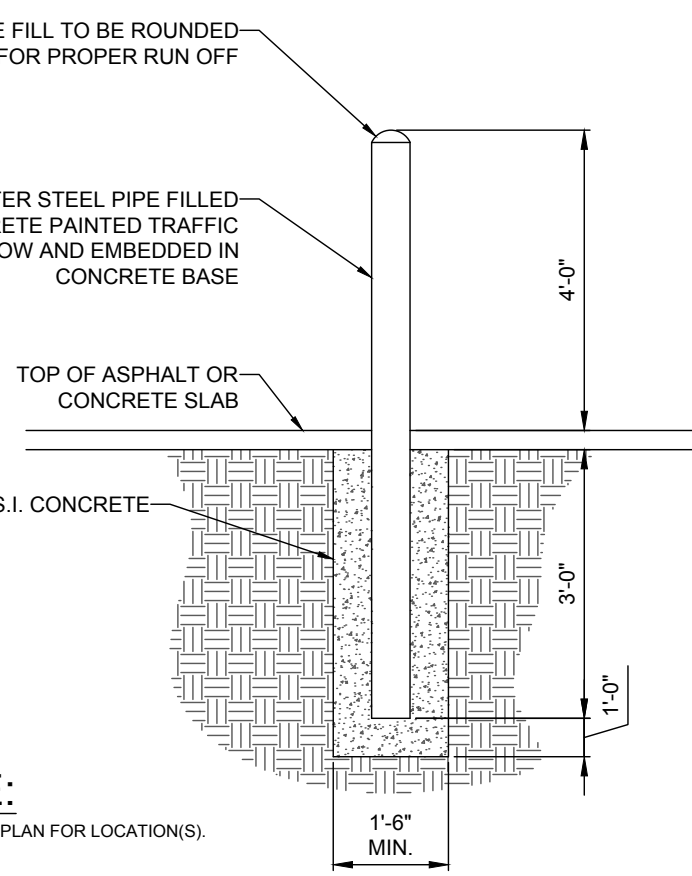
ADA ACCESSIBLE RAMP DETAIL
NOT TO SCALE



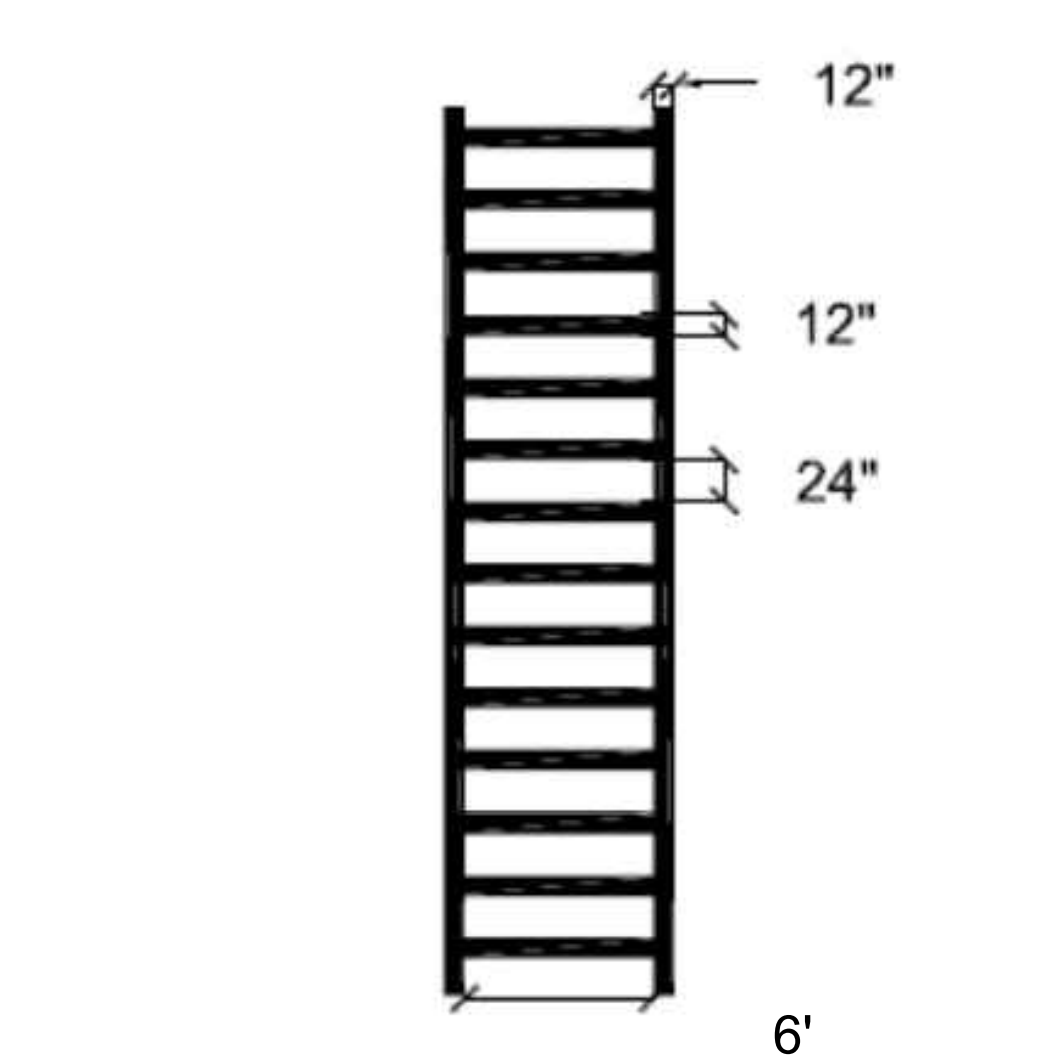
STANDARD STOP LINE GRAPHIC DETAIL
NOT TO SCALE



CURB TAPER DETAIL
NOT TO SCALE



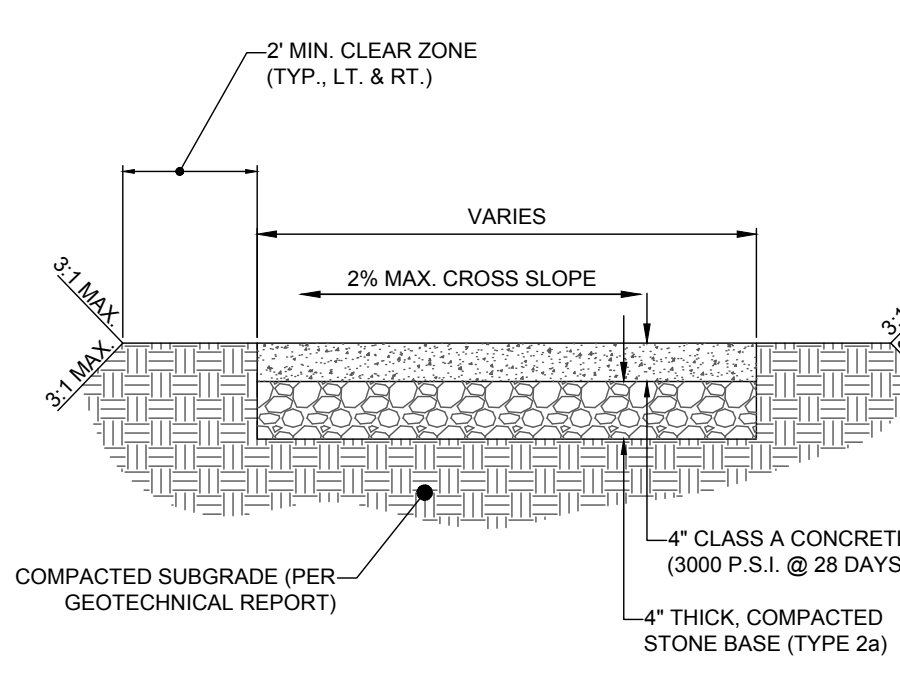
BOLLARD DETAIL
NOT TO SCALE



LONGITUDINAL CROSSWALK DETAIL
NOT TO SCALE

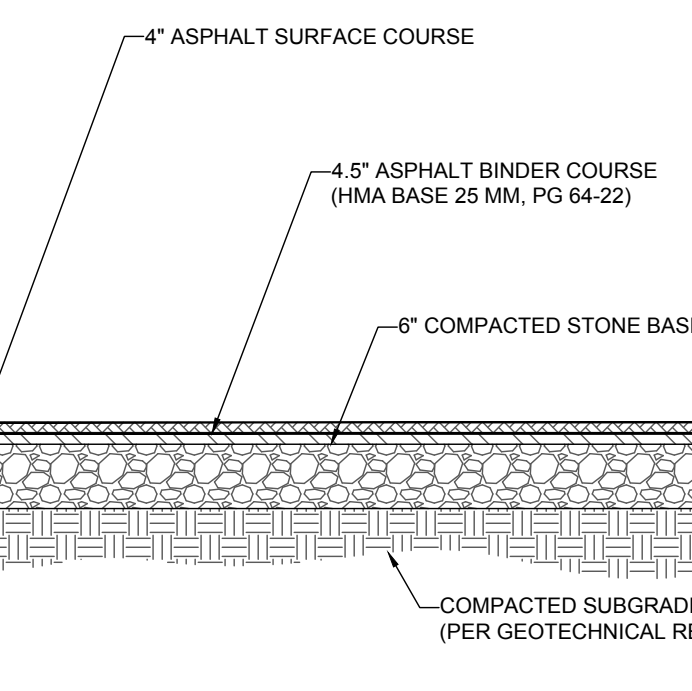


"STOP SIGN" DETAIL 'R1-1'
NOT TO SCALE



NOTE:
1. EXPANSION JOINTS 1/2\"/>

CONCRETE SIDEWALK DETAIL
NOT TO SCALE



STANDARD DUTY ASPHALT PAVEMENT SECTION
NOT TO SCALE

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REVISIONS

REV	DATE	COMMENT	DRAWN BY	CHECKED BY

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NOT APPROVED FOR CONSTRUCTION

PROJECT No.: MB202020
DRAWN BY: AVB
CHECKED BY: NBS / A.J.L.
DATE: 02/22/2021
CAD ID:

DETAILED SITE PLAN
FOR
TDC PARCEL N INVESTOR, LLC
PROPOSED DEVELOPMENT
6400 AMERICA BLVD
PRINCE GEORGE'S COUNTY
HYATTSVILLE, MARYLAND
TM: 42, GRID: A2

BOHLER
16701 MELFORD BLVD., SUITE 310
BOWIE, MARYLAND 20715
Phone: (301) 809-4500
Fax: (301) 809-4501
MD@BohlerEng.com

NICHOLAS B. SPEACH
PROFESSIONAL ENGINEER
LICENSE NO. 42083, EXPIRATION DATE: 6/14/2023

SHEET TITLE:
SITE DETAILS
SHEET NUMBER:
DSP-8
ORG. DATE - 02/22/2021

UNIVERSITY TOWN CENTER PARCEL N

6400 America Blvd, Hyattsville
Prince Georges County, Maryland

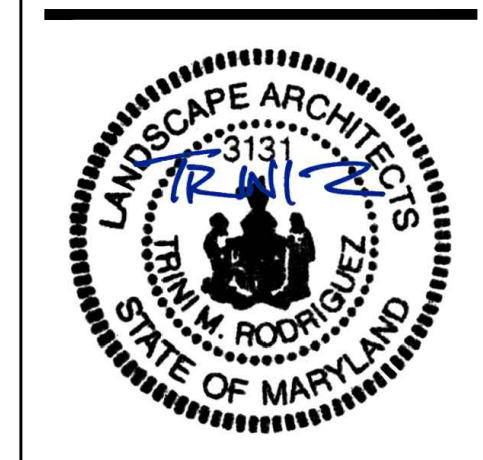
LANDSCAPE ARCHITECT
ParkerRodriguez

101 North Union Street, Suite 320
Alexandria VA 22314
703.548.5010

OWNER
TDC Parcel N Investor, LLC

ARCHITECT
BCT Design Group
100 North Charles Street
Baltimore, MD 21201
410.837.2727

CIVIL ENGINEER
Bohler Engineering
16701 Melford Boulevard, Suite 310
Bowie MD 20715
301.809.4500



REVISIONS

DSP-21006	04.30.2021

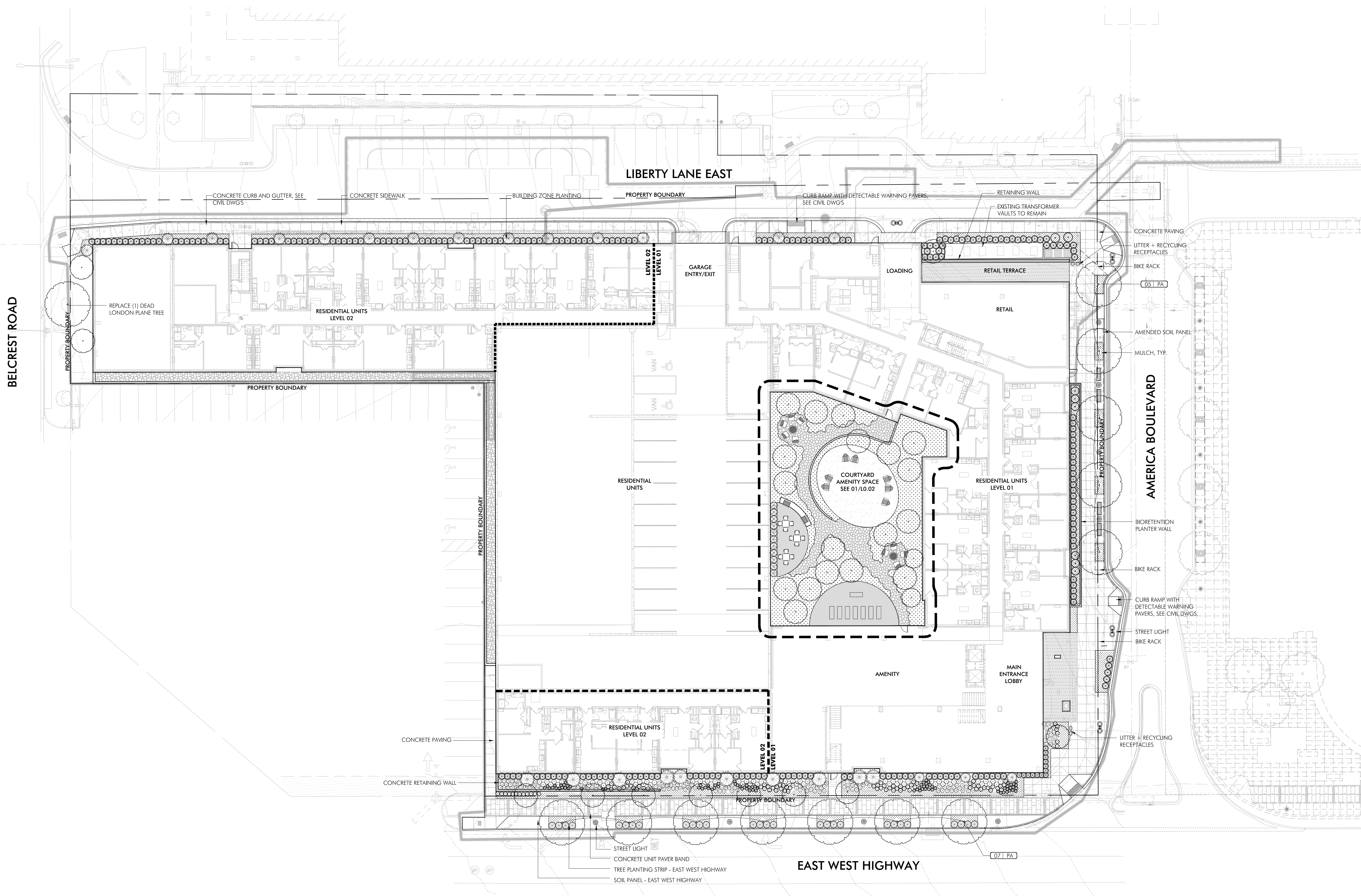
DESIGNED BY
CM/TR

DRAWN BY
SS/IB

CHECKED BY
TR

LANDSCAPE PLAN - STREETScape

L0.01



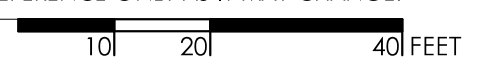
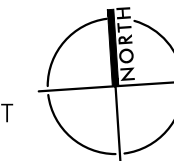
LANDSCAPE DRAWING LIST

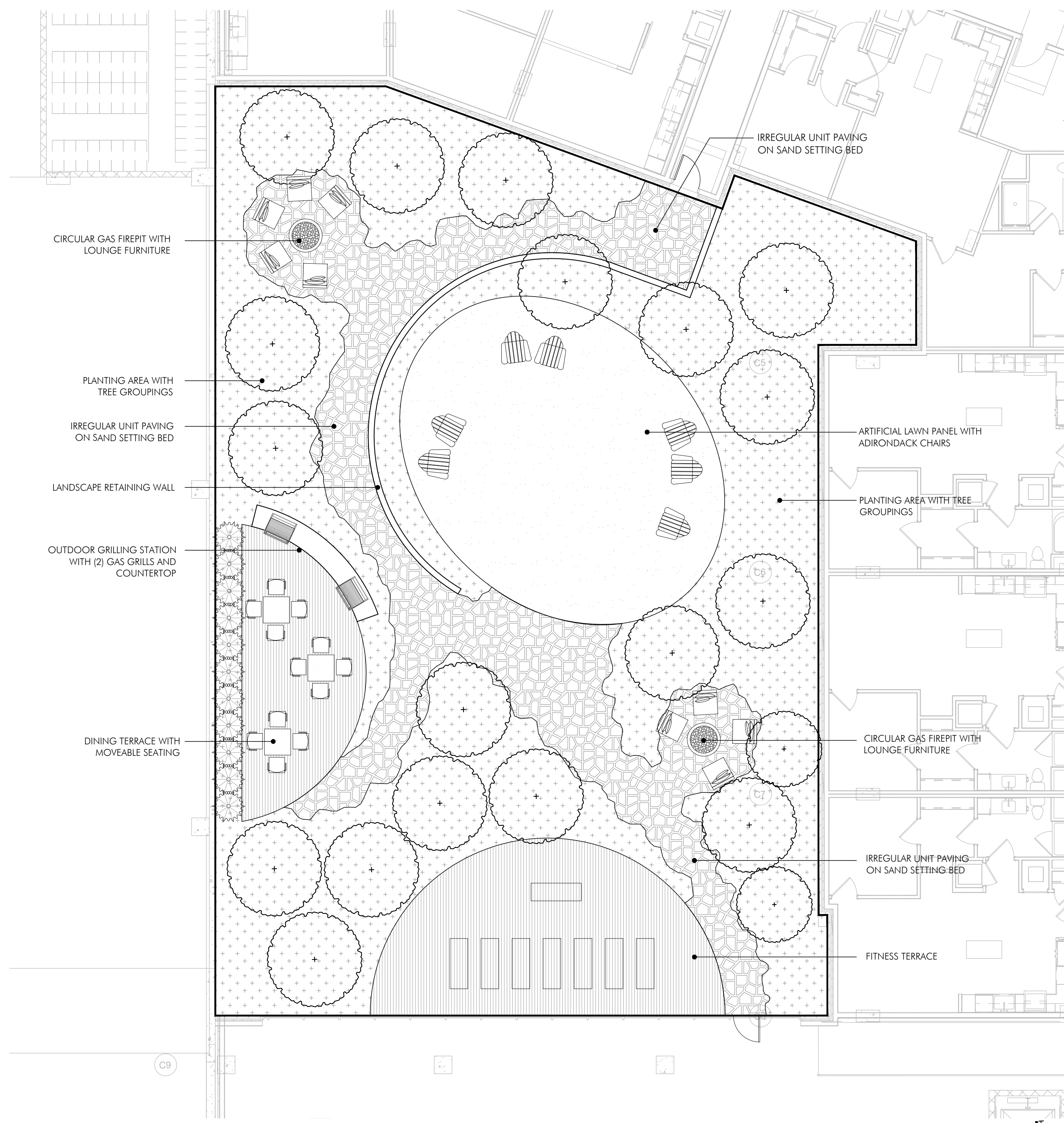
L0.01	DSP LANDSCAPE PLAN - STREETScape
L0.02	DSP LANDSCAPE PLANS - AMENITIES
L0.10	HARDSCAPE DETAILS
L0.20	PLANTING DETAILS
L0.21	PLANTING NOTES + SCHEDULES

01 OVERALL LANDSCAPE PLAN

1"=20'-0"

NOTE: INTERIOR ARCHITECTURE IS SHOWN FOR GENERAL REFERENCE ONLY AS IT MAY CHANGE.



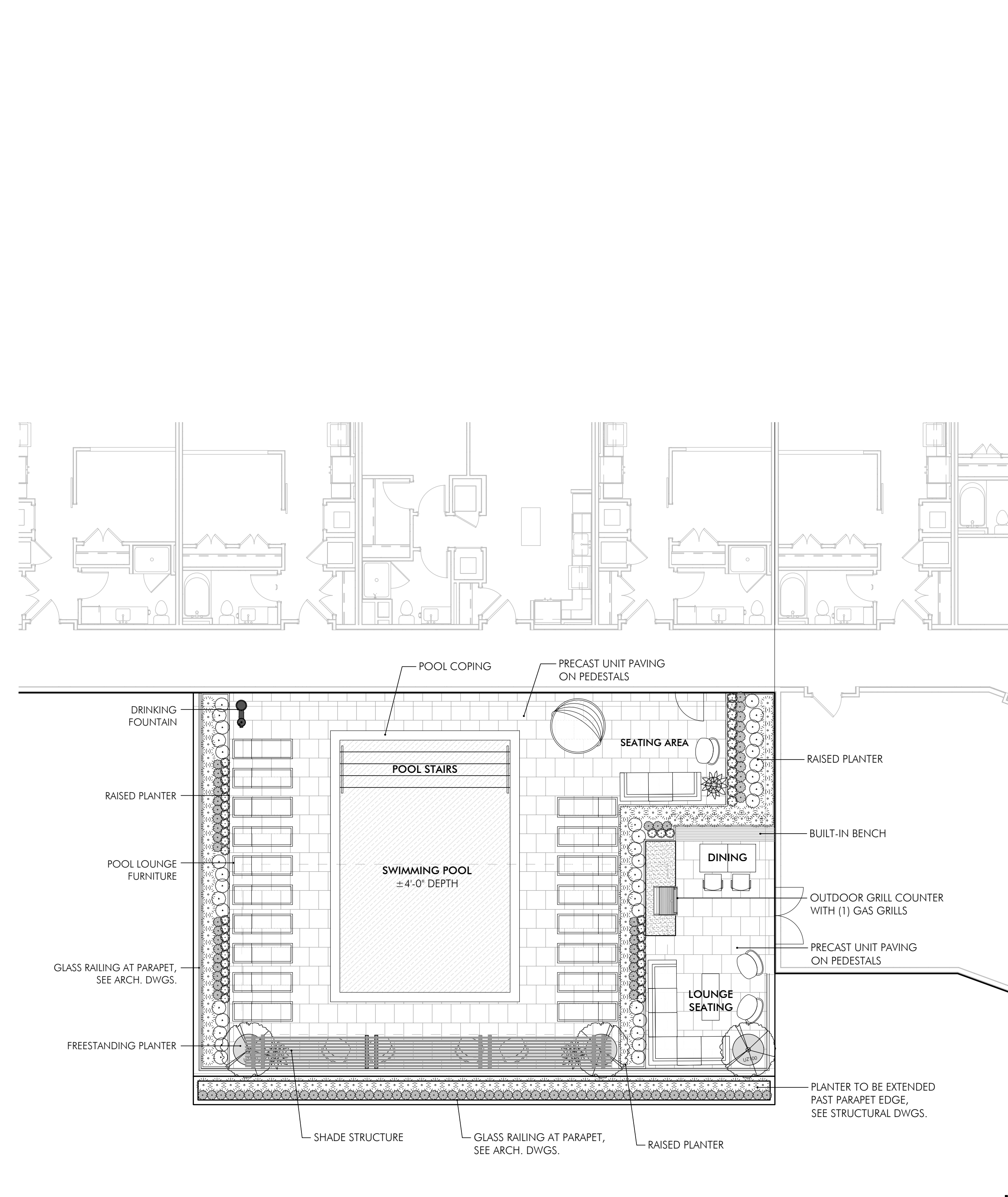


01 LANDSCAPE PLAN - COURTYARD

1/8"=1'-0"

NOTE: INTERIOR ARCHITECTURE IS SHOWN FOR GENERAL REFERENCE ONLY AS IT MAY CHANGE.

4 8 16 FEET



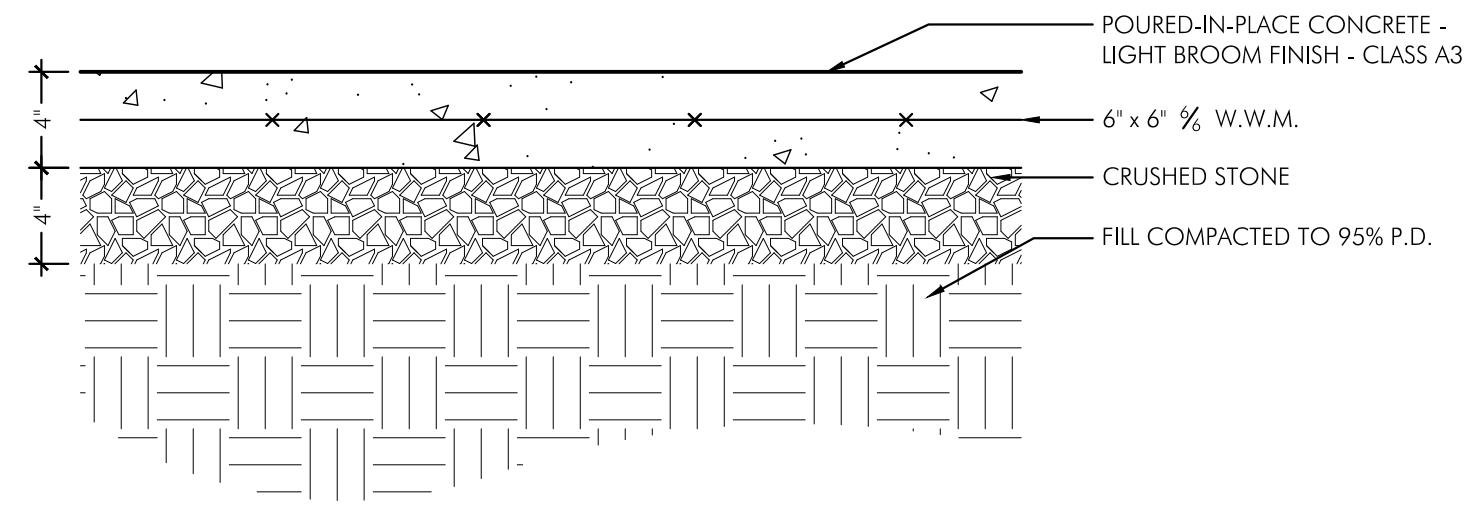
02 LANDSCAPE PLAN - LEVEL 07

1/8"=1'-0"

NOTE: INTERIOR ARCHITECTURE IS SHOWN FOR GENERAL REFERENCE ONLY AS IT MAY CHANGE.

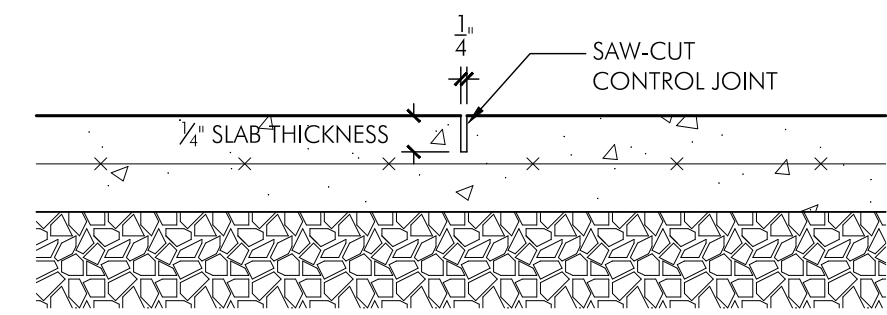
4 8 16 FEET





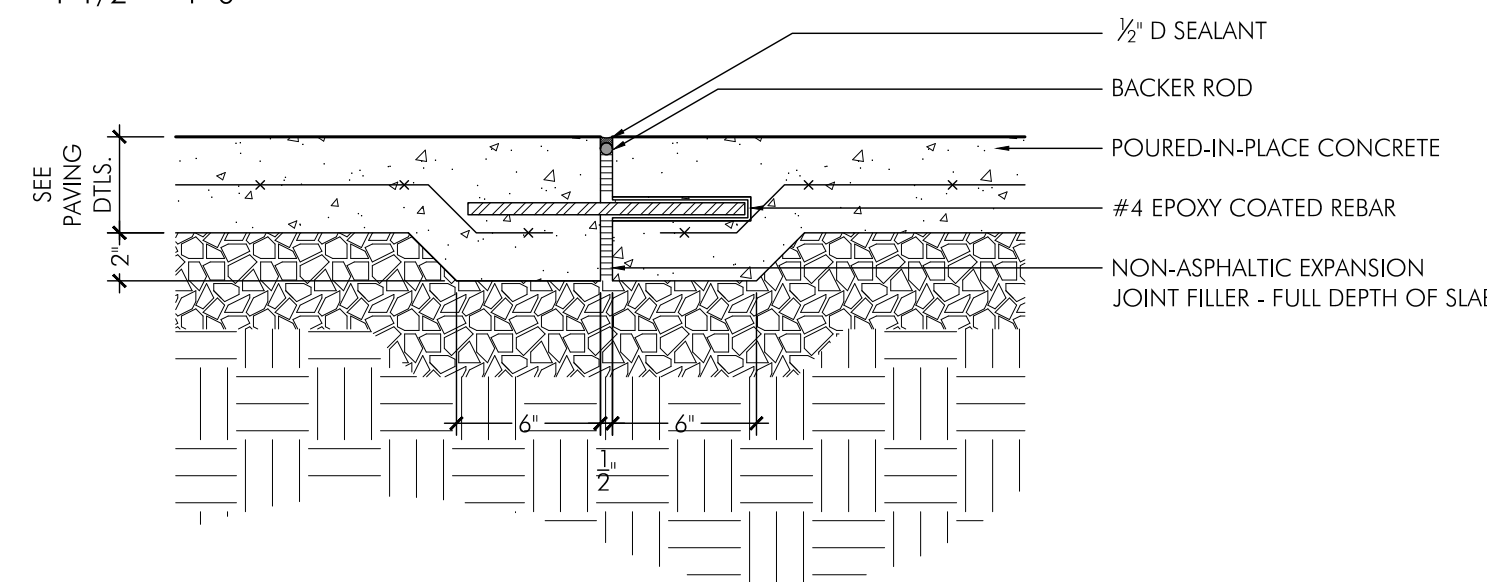
01 CONCRETE PAVING

1 1/2" = 1'-0"



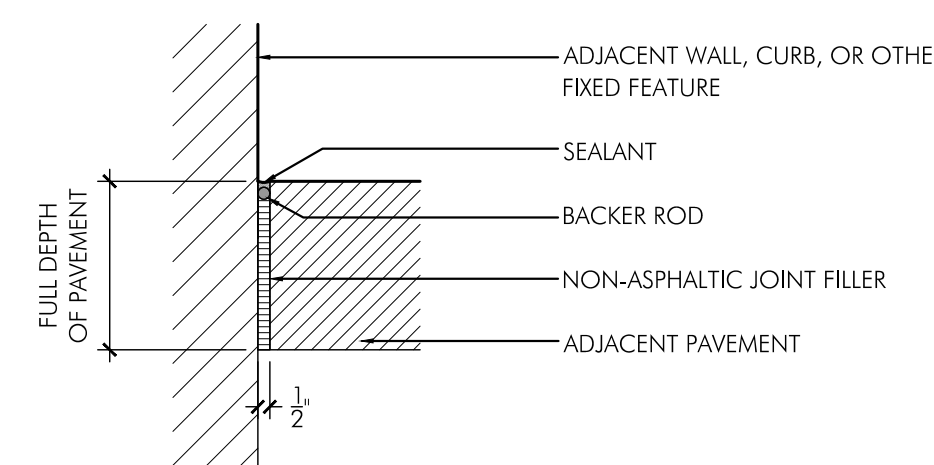
02 CONTROL JOINT IN CONCRETE PAVING

1 1/2" = 1'-0"



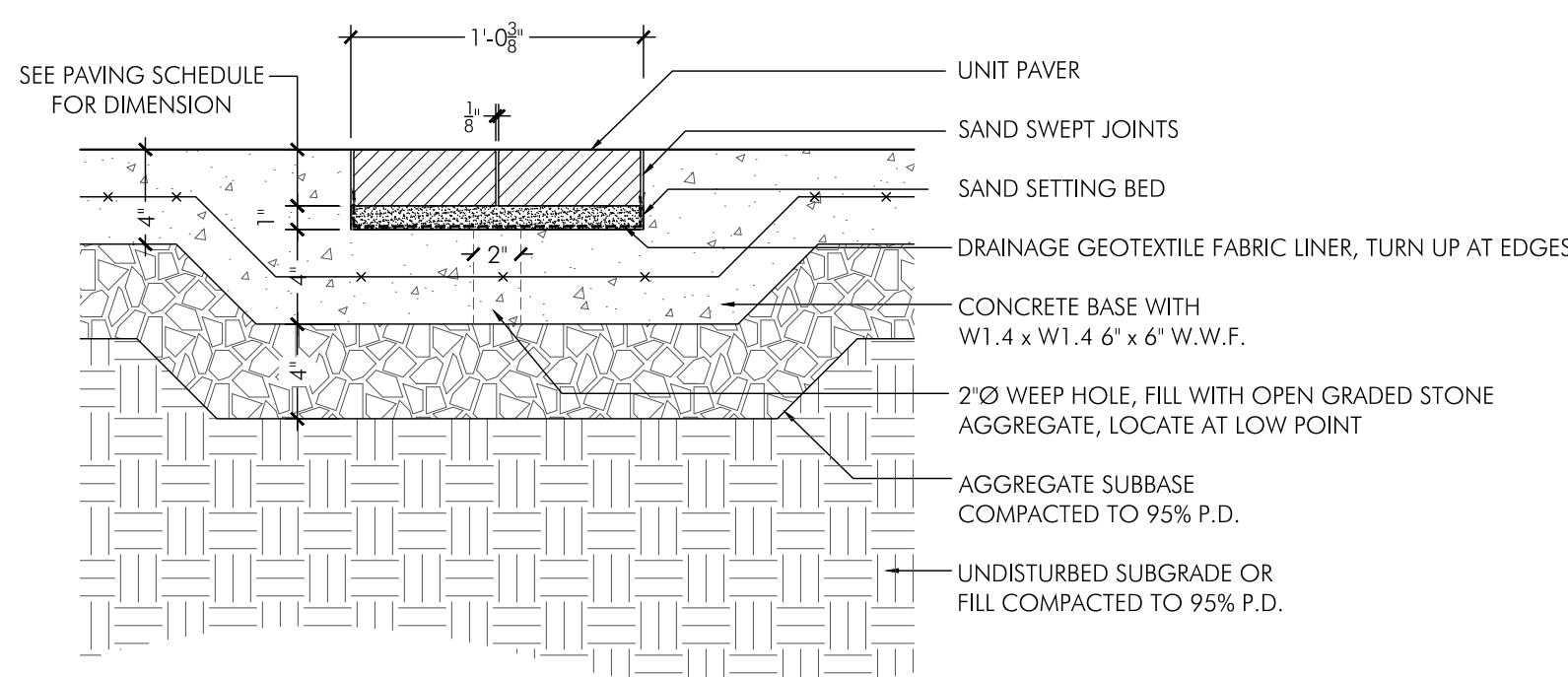
03 EXPANSION JOINT IN CONCRETE PAVING

1 1/2" = 1'-0"



04 ISOLATION JOINT

1 1/2" = 1'-0"



05 CONCRETE UNIT PAVING - PEDESTRIAN

1 1/2" = 1'-0"



06 BENCH

NOT TO SCALE

MANUFACTURER: LANDSCAPE FORMS
www.landscapeforms.com
Tel. 800.430.6209
MODEL: NEOCOMBO OR APPROVED EQUAL BACKED STYLE, WITH END ARMS, 69\"/>



07 BIKE RACK

NOT TO SCALE

MANUFACTURER: LANDSCAPE FORMS
www.landscapeforms.com
Tel. 800.430.6209
MODEL: RIDE, OR APPROVED EQUAL SURFACE MOUNT
OPTIONS:



08 LITTER RECEPTACLE

NOT TO SCALE

MANUFACTURER: LANDSCAPE FORMS
www.landscapeforms.com
Tel. 800.430.6209
MODEL: METRO LITTER RECEPTACLE OR APPROVED EQUAL TOP OPENING; 30 GAL. CAPACITY; WITH KEYED LOCK SURFACE MOUNT
OPTIONS:



09 RECYCLING RECEPTACLE

NOT TO SCALE

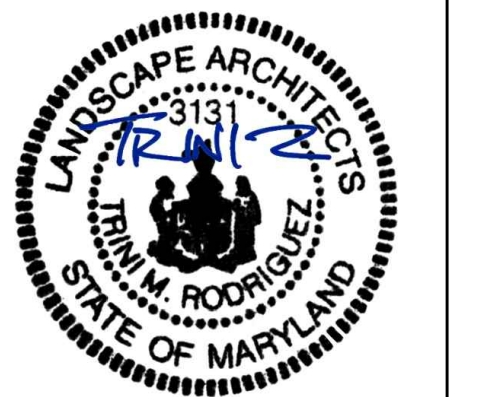
MANUFACTURER: LANDSCAPE FORMS
www.landscapeforms.com
Tel. 800.430.6209
MODEL: METRO RECYCLING RECEPTACLE OR APPROVED EQUAL TOP OPENING; 30 GAL. CAPACITY; WITH KEYED LOCK SURFACE MOUNT
OPTIONS:



REVISIONS

DSP-21006	04.30.2021

DESIGNED BY CM/TR
DRAWN BY SS/JB
CHECKED BY TR



REVISIONS

DSP-21006 04.30.2021

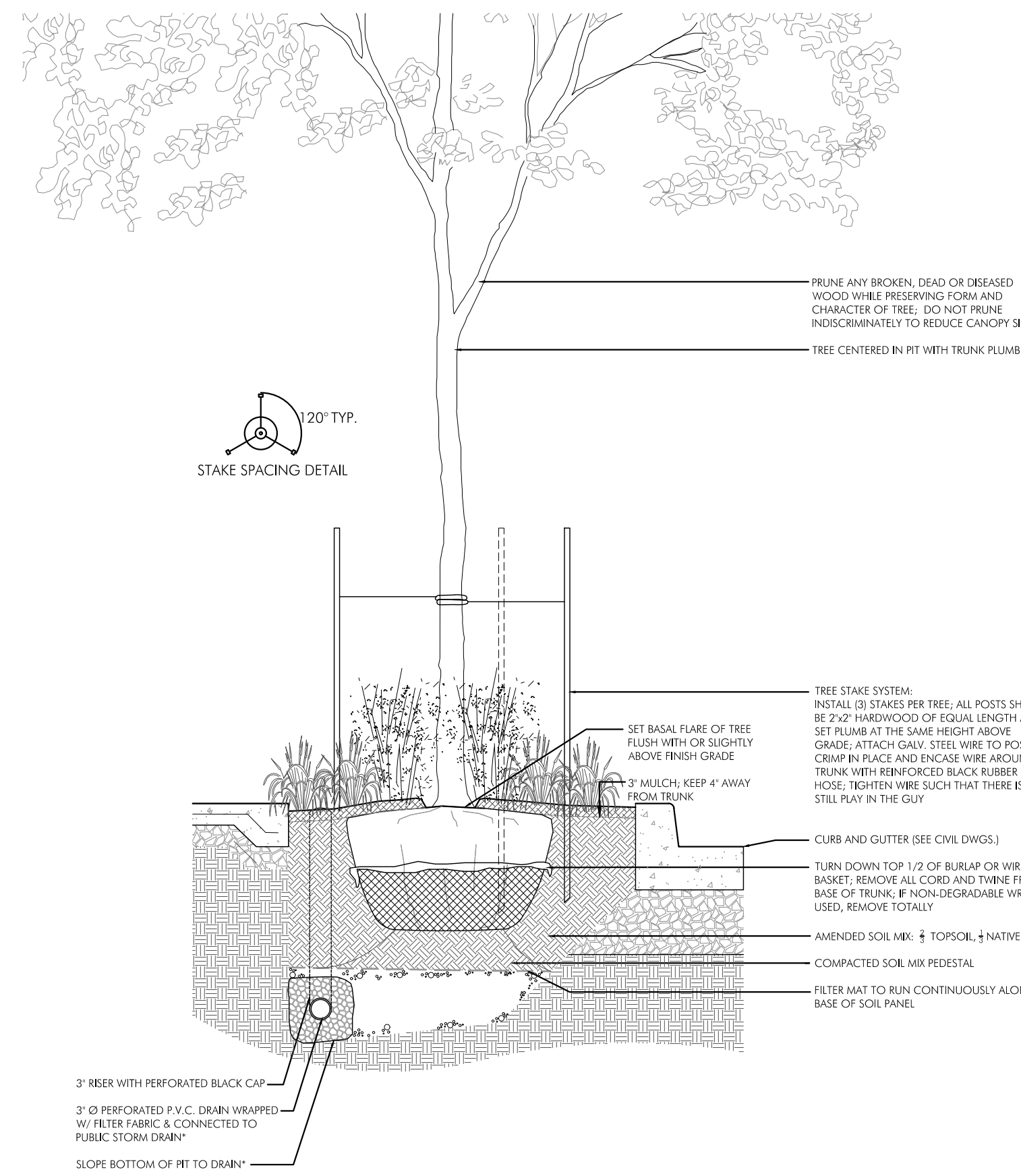
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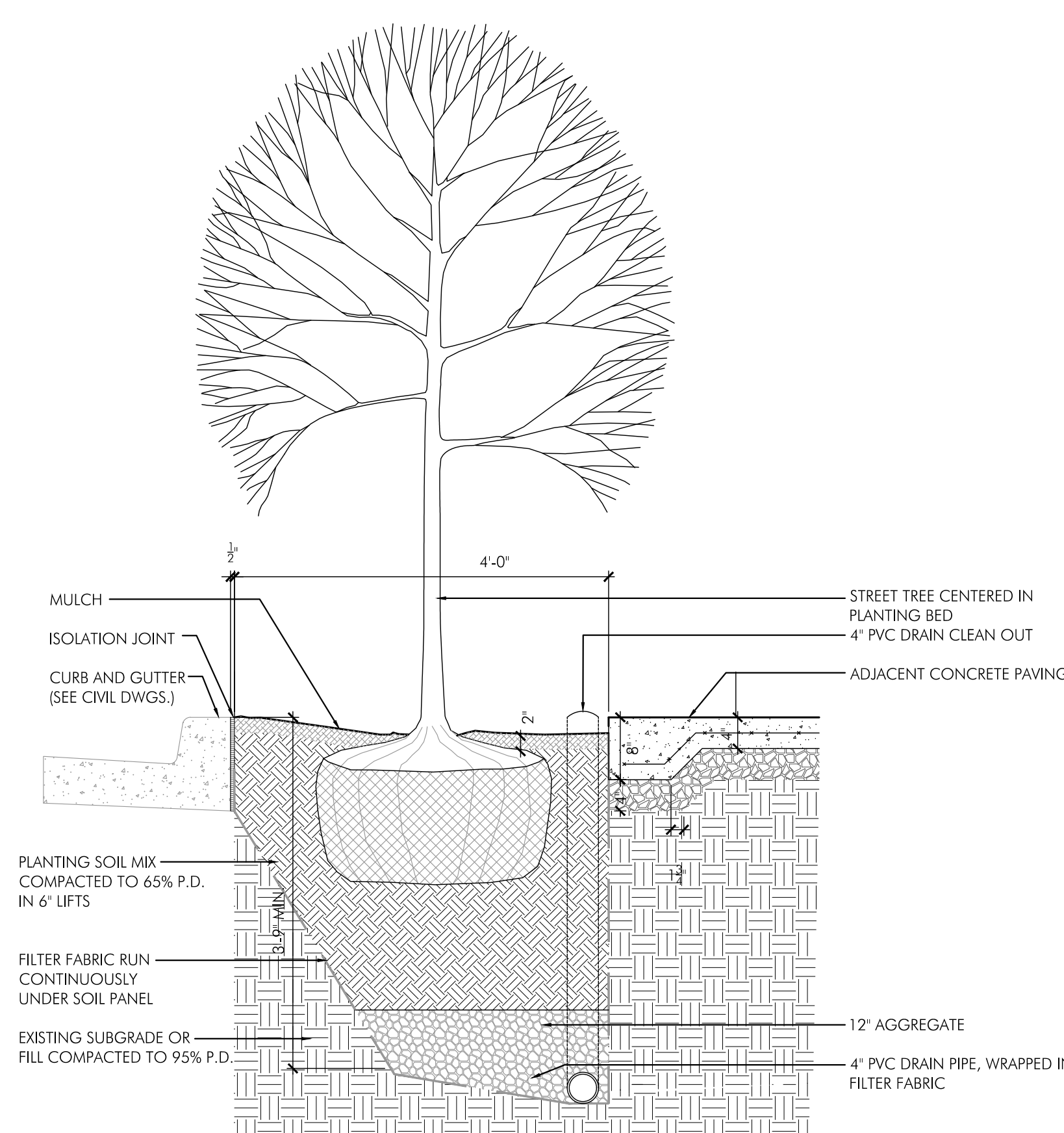
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PLANTING
DETAILS

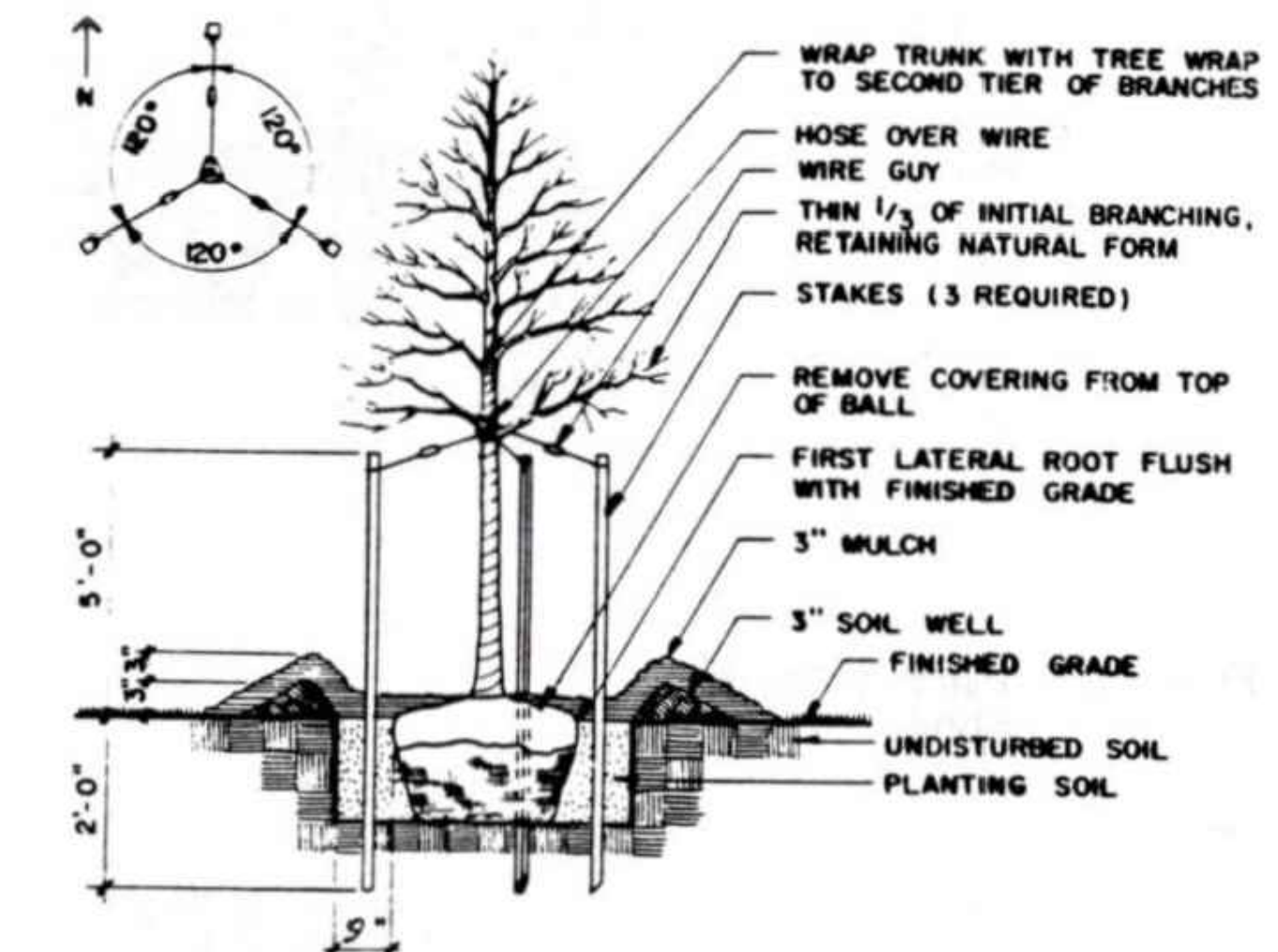
L0.20



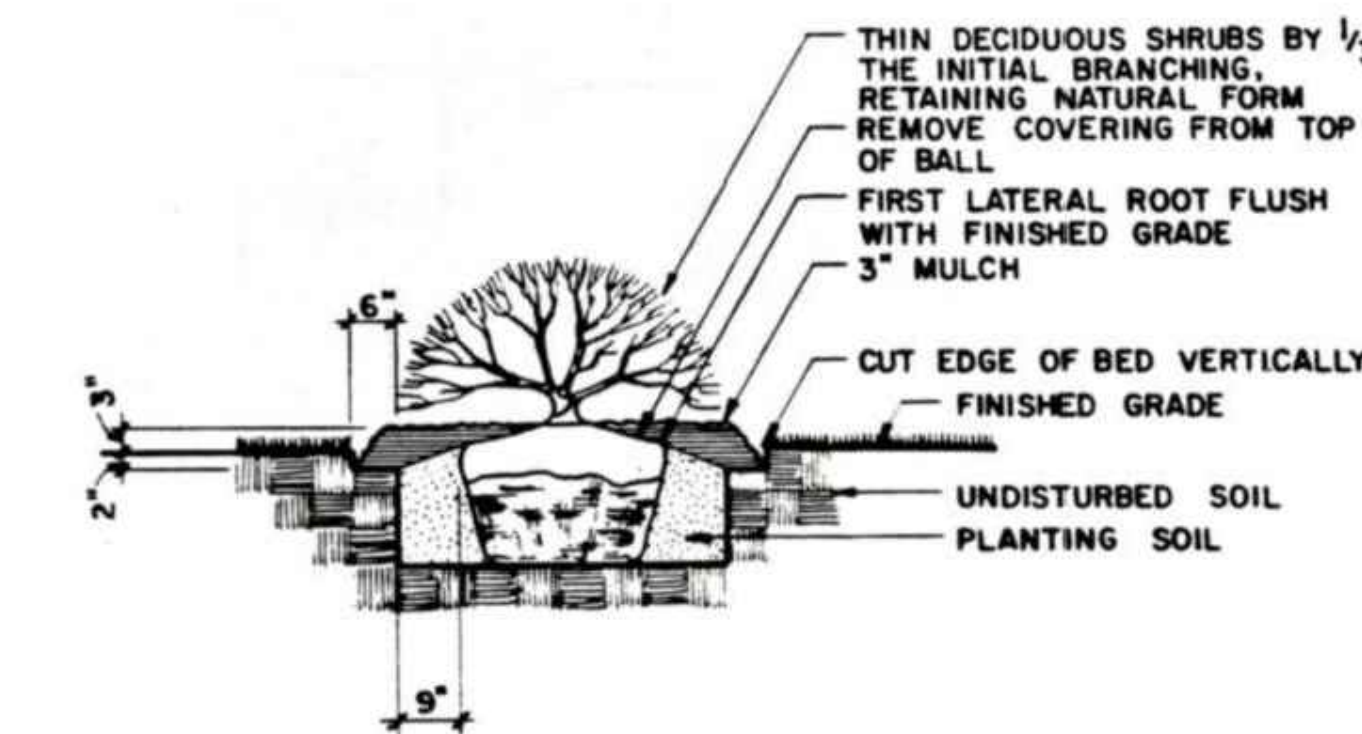
01 TREE PIT - EAST WEST HIGHWAY
NOT TO SCALE



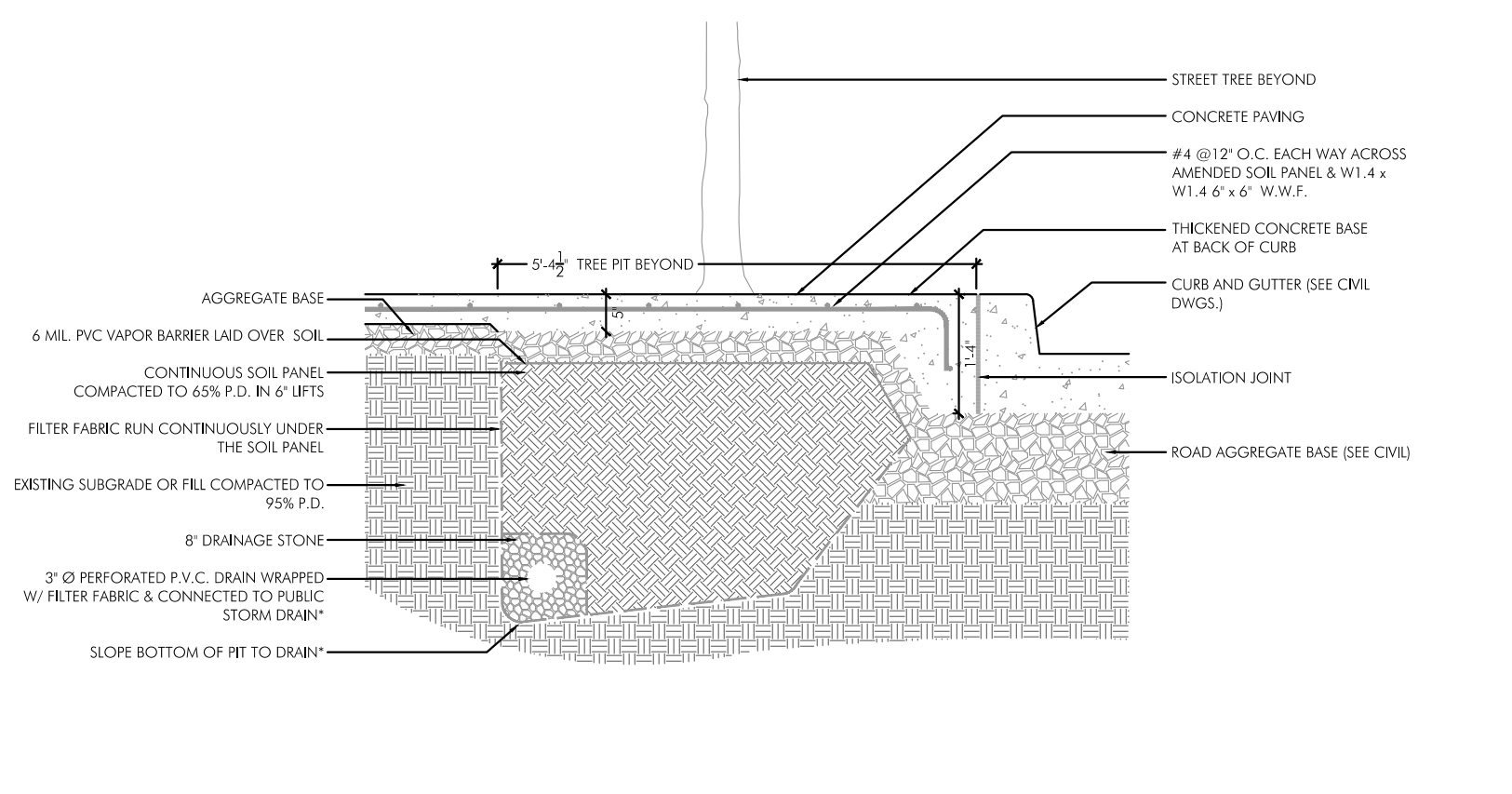
03 TREE PIT - AMERICA BOULEVARD
NOT TO SCALE



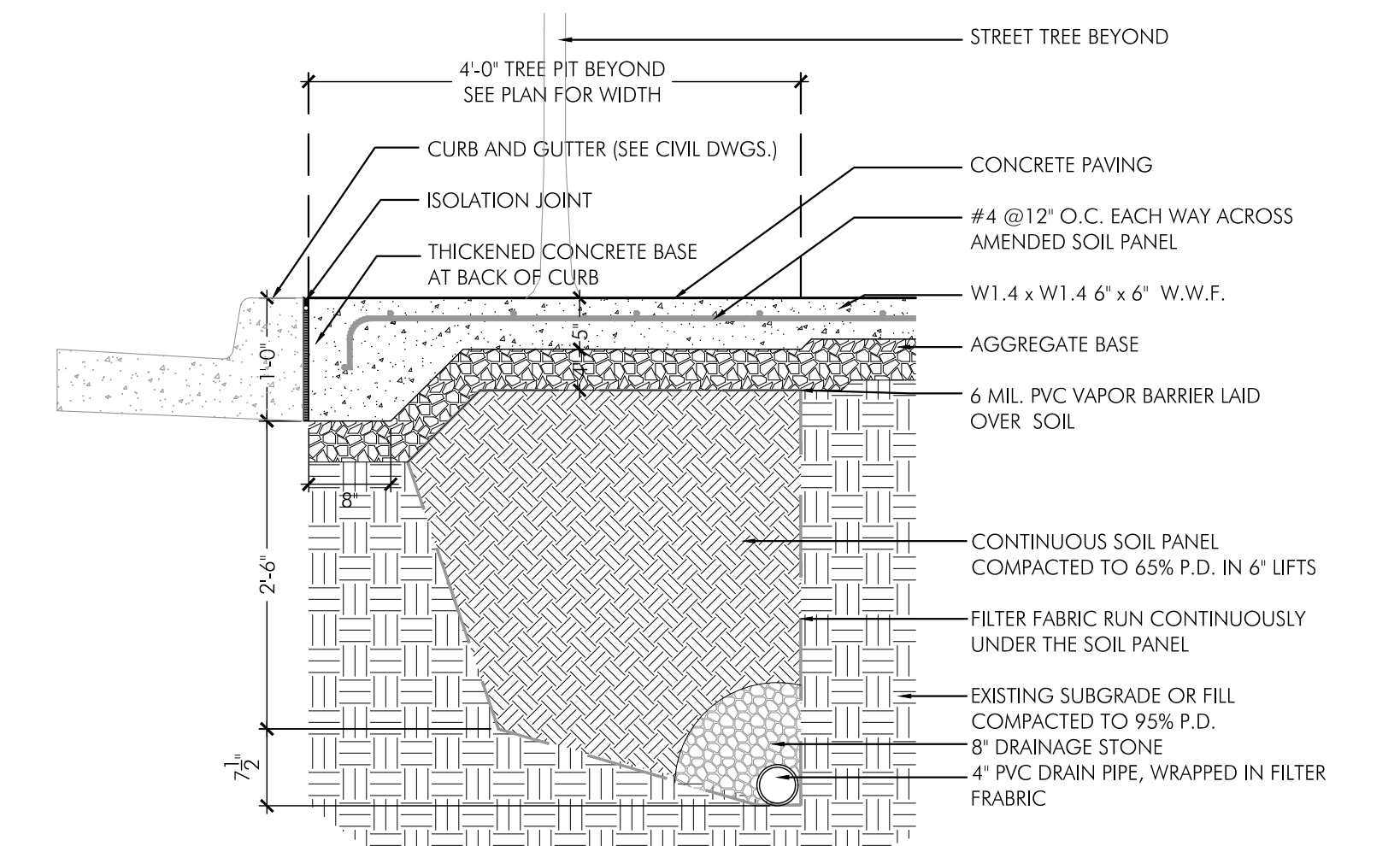
06 TYP. DECIDUOUS TREE PLANTING DETAIL
NOT TO SCALE



07 TYP. SHRUB PLANTING DETAIL
NOT TO SCALE



02 SOIL PANEL - EAST WEST HIGHWAY
NOT TO SCALE



04 SOIL PANEL - AMERICA BOULEVARD
NOT TO SCALE

PLANT SPACING 'D'	ROW 'A'
6' O.C.	5' O.C.
8' O.C.	7' O.C.
10' O.C.	9' O.C.
12' O.C.	10' O.C.
14' O.C.	12' O.C.
15' O.C.	13' O.C.
16' O.C.	14' O.C.
18' O.C.	16' O.C.
24' O.C.	21' O.C.
30' O.C.	26' O.C.
36' O.C.	30' O.C.

NOTE:
GROUND COVERS AND PERENNIALS TO BE INSTALLED WITH TRIANGULAR SPACING

05 GROUND COVER SPACING
NOT TO SCALE

PLANTING NOTES

1. The Landscape Contractor is to submit proof of qualifications for review by the Owner's Representative with the bid. The Landscape Contractor's qualifications must include a reference list of at least 5 landscape installations using plant material of a similar size, quantity, and magnitude of work to the current project. Project references are to be provided with the dollar amount of the project, date of completion, and phone numbers of contacts at each location.

2. The Landscape Contractor shall furnish, deliver, and install plant material. Plant material shall be healthy, vigorous, free from plant diseases and insects.

3. Plant material shall be nursery stock grown in Maryland, Delaware, New Jersey, Ohio, Pennsylvania, Virginia or West Virginia for at least two years prior to digging.

4. The Landscape Contractor shall notify the Owner's Representative to select and seal plant material at the nursery and to stake plant material locations at the site before delivery of plants to site.

5. Inspection of Planting Beds - the Landscape Contractor shall inspect planting areas before topsoiling or planting are begun to ensure that adequate drainage exists. If areas to be landscaped show evidence of poor drainage, the Landscape Contractor shall notify the Owner's Representative immediately for corrective action. Plant material that dies due to poor or inadequate drainage shall be the responsibility of the Landscape Contractor. The Owner's Representative reserves the right to make changes or substitutions in plant type or quantities for the purposes of insuring proper plant growth. Landscape Contractor may not make any substitutions without approval from the Owner's Representative.

6. Shrubs and ground cover beds shall be mulched to a depth of 3" min. Mulch shall be shredded hardwood bark, aged max. 6 months.

7. Trees, shrubs and ground cover shall be planted as specified and installed in accordance with A.A.N. Standards and the details and comments noted on the drawings.

8. All planting shall be performed by personnel familiar with planting procedure and under the supervision of a qualified planting foreman.

9. All landscape work shall be coordinated with other trades to prevent conflicts.

10. Proposed and relocated plant material shall be guaranteed for one (1) calendar year from date of acceptance by Owner's Representative.

11. On-site topsoil or imported topsoil from same source as topsoil used on site shall be used for planting and finish grading. Topsoil must be free of plant parts, clay lumps, stones, or similar objects larger than 1".

12. All topsoil shall be tested by a recognized laboratory for pH and soluble salts. A pH of 4.5 to 7.5 is required. Soluble salts shall not be higher than 500 parts per million.

13. Backfill material around tree balls shall be a mixture by volume of the following materials in quantities specified: 4/5 existing clean topsoil and 1/5 organic compost. Trees are to be fertilized on the soil surface at a rate of 1 lb. of nitrogen/1000s.f. of soil surface area.

14. Water all plants immediately after planting sufficient to thoroughly moisten all of the backfilled earth. Plants shall be kept in a moistened but not saturated condition for the duration of the guarantee period by the Landscape Contractor. Where water is not available on site, the Landscape Contractor shall furnish sufficient quantities to complete the work at no cost to the Owner's Representative.

15. All broken or damaged roots shall be cut off smoothly and the tops of all trees shall be pruned in a manner complying with standard horticultural practice. At the time pruning is completed, all remaining wood shall be alive. Fine pruning for tree shape and appearance shall be done only under the direction of the Owner's Representative. At the end of the guarantee period at least 80 percent of the wood remaining shall be alive.

16. The Landscape Contractor shall maintain plantings prior to the beginning of the guarantee period by watering, fertilizing, disease control, pruning, weeding, and replacement of dead, stolen, or unacceptable materials, etc., so as to keep the completed work and/or incomplete work in clean and neat condition at all times.

17. The Landscape Contractor shall give written notice to the Owner's Representative requesting an inspection to begin the one year guarantee period at least ten days prior to the anticipated date of completion.

18. After review, the Landscape Contractor will be notified of the date that the work has been approved for beginning the guarantee period or, if there are any deficiencies, a list of work items to be corrected prior to the beginning of the guarantee period.

19. The Landscape Contractor shall notify Owner's Representative at least 10 days prior to the end of the guarantee and such guarantee shall be extended until notification is received. The Landscape Contractor shall guarantee that plants shall be in vigorous and thriving condition and request full inspection prior to final acceptance of work. At the end of the guarantee period, all plants that are dead or show unsatisfactory growth shall be replaced within one month.

20. Within the guarantee period, the Landscape Contractor will notify the Owner's Representative of any maintenance practices being followed or omitted which would be detrimental to the health of the plants.

21. The Landscape Contractor shall be responsible for the removal of all stakes and/or guys after the second growing season following planting.

BUFFERING INCOMPATIBLE USES

Schedule 4.7-1	
Buffering Incompatible Uses Requirements	
1) General Plan designation:	<input checked="" type="checkbox"/> Developed Tier, Corridor Node or Center <input type="checkbox"/> Developing or Rural Tier
2) Use of proposed development:	<input type="checkbox"/> MEDIUM
3) Impact of proposed development:	<input type="checkbox"/> MEDIUM
4) Use of adjoining development:	<input type="checkbox"/> MEDIUM
5) Impact of adjoining development:	<input type="checkbox"/> MEDIUM
6) Minimum required bufferyard (A, B, C, D or E):	<input type="checkbox"/> BUFFER NOT REQUIRED
7) Minimum required building setback:	<input type="checkbox"/> feet
8) Building setback provided:	<input type="checkbox"/> feet
9) Minimum required width of landscape yard:	<input type="checkbox"/> feet
10) Width of landscape yard provided:	<input type="checkbox"/> feet
(The required setback and landscape yard may be reduced by fifty percent (50%) in the Developed Tier, Corridor Node or Center when a six (5) foot high fence or wall is provided.)	
11) Linear feet of buffer strip required along property line and right-of-way:	<input type="checkbox"/> linear
12) Percentage or required bufferyard occupied by existing trees:	<input type="checkbox"/> %
13) Is a six (6) foot high fence or wall included in bufferyard?	<input type="checkbox"/> yes <input type="checkbox"/> no
(The required plant material may be reduced by fifty percent (50%) when a six (6) foot high fence or wall is provided.)	
14) Total number of plant units required in buffer strip:	<input type="checkbox"/> p.u.
15) Total number of plant units provided:	shade trees <input type="checkbox"/> x 10 p.u. = <input type="checkbox"/> p.u. evergreen trees <input type="checkbox"/> x 5 p.u. = <input type="checkbox"/> p.u. ornamental trees <input type="checkbox"/> x 5 p.u. = <input type="checkbox"/> p.u. shrubs <input type="checkbox"/> x 1 p.u. = <input type="checkbox"/> p.u.

SUSTAINABLE LANDSCAPING

Schedule 4.9-1	
Sustainable Landscaping Requirements	
1) Percentage of native plant material required in each category:	
Shade Trees:	total <input type="checkbox"/> x 50% = <input type="checkbox"/> total number required total number provided = <input type="checkbox"/> % native
Ornamental Trees:	total <input type="checkbox"/> x 50% = <input type="checkbox"/> total number required total number provided = <input type="checkbox"/> % native
Evergreen Trees:	total <input type="checkbox"/> x 30% = <input type="checkbox"/> total number required total number provided = <input type="checkbox"/> % native
Shrubs:	total <input type="checkbox"/> x 30% = <input type="checkbox"/> total number required total number provided = <input type="checkbox"/> % native
2) Are invasive species proposed?	<input type="checkbox"/> yes <input checked="" type="checkbox"/> no
3) Are existing invasive species on-site in areas that are to remain undisturbed?	<input type="checkbox"/> yes <input checked="" type="checkbox"/> no
4) If "yes" is checked in numbers 2 or 3, is a note included on the plan requiring removal of invasive species prior to certification in accordance with Section 1.5, Certification of Installation of Plant Materials?	<input type="checkbox"/> yes <input type="checkbox"/> no
5) Are trees proposed to be planted on slopes greater than 3:1?	<input type="checkbox"/> yes <input checked="" type="checkbox"/> no

PLANT SCHEDULE - GROUND LEVEL

Key	Qty.	Botanical name	Common name	Stock size (height/caliper)	Stock type	Comments
Canopy Trees						
CB	10	<i>Carpinus betulus</i>	European hornbeam	1' - 1 1/2' cal.	B&B	single leader; full branching
PA	13	<i>Platanus x acerifolia 'Bloodgood'</i>	London Planetree	2' - 2 1/2' cal.	B&B	single leader; full branching
Ornamental Trees						
AC	5	<i>Amelanchier canadensis</i>	Serviceberry	8' -10' ht.	B&B	single stem; full branching
CC	5	<i>Cercis canadensis</i>	Eastern Redbud	8' -10' ht.	B&B	single stem; full branching
LI	5	<i>Lagerstroemia indica 'Muskogee'</i>	Muskogee Crape Myrtle	8' -10' ht.	B&B	multistem-3 stems minimum; full branching
MV	10	<i>Magnoia virginiana</i>	Sweetbay Magnolia	8' -10' ht.	B&B	multistem-3 stems minimum; full branching
Shrubs						
IC		<i>Ilex crenata 'Hoogendorn'</i>	Hoogendorn Japanese Holly	18-24" ht.	Cont.	full branching
IG		<i>Ilex glabra 'Compacta'</i>	Compact Inkberry	30-36" ht.	Cont.	full branching to ground
PL		<i>Prunus laurocerasus 'Otto Luykens'</i>	Otto Luykens Cherry Laurel	30-36" ht.	Cont.	full branching to ground
RO		<i>Rosa 'Knockout'</i>	Knockout Rose	18-24" ht.	Cont.	full branching;
Herbaceous Perennials, Ornamental Grasses, and Ground Covers						
NTE		<i>Nassella tenuissima</i>	Mexican Feather Grass	2 gal.	Cont.	18" o.c.
MCA		<i>Muhlenbergia capillaris</i>	Pink Muhly Grass	2 gal.	Cont.	24" o.c.
SSB		<i>Schizachyrium scoparium 'Standing Ovation'</i>	Standing Ovation Little Bluestem	2 gal.	Cont.	18" o.c.
LMB		<i>Liriope muscari</i>	Lilyturf	1 gal.	Cont.	15" o.c.
NWL		<i>Nepeta x faassenii 'Walker's Low'</i>	Catnip	1 gal.	Cont.	18" o.c.
SAT		<i>Sesleria autumnalis</i>	Autumn Moor Grass	1 gal.	Cont.	18" o.c.
SNM		<i>Salvia nemerosa 'Mainachf'</i>	May Night Salvia	1 gal.	Cont.	18" o.c.
Vines						
HA		<i>Hydrangea anomala petiolaris</i>	Climbing Hydrangea	2 gal.	Cont.	accent
Bulbs						
AGI		<i>Allium 'Giganteum'</i>	Giant Allium		bulbs	accent
Seasonal Planting						
ANN		Seasonal Planting- to be selected by owner				
NOTE:						
All materials' specifications shall be in accordance with the industry standard for grading plant material - The American Standard for Nursery Stock (ANSI Z60.1).						
Maintenance of all trees and landscape materials shall conform to accepted industry standards set forth by the Landscape Contractors Association, American						

UNIVERSITY TOWN CENTER PARCEL N

6400 America Blvd, Hyattsville
Prince Georges County, Maryland

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Alexandria VA 22314
703.548.5010

OWNER
TDC Parcel N Investor, LLC

ARCHITECT
BCT Design Group
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Baltimore MD 21201
410.837.2727

CIVIL ENGINEER
Bokler Engineering
16701 Mellford Boulevard, Suite 310
Bowie MD 20715
301.809.4500



REVISIONS

DSP-21006 04.30.2021

DESIGNED BY

CM/TR

DRAWN BY

SS/JB

CHECKED BY

TR

PLANTING NOTES + SCHEDULES

L0.21

I. INTRODUCTION

TDC Parcel N Investor, LLC (the “**Applicant**”) by and through its attorneys, Lerch, Early & Brewer, Chartered, submits this Detailed Site Plan (“**DSP**”) Justification Statement (the “**Statement**”) to demonstrate that the proposed mixed-use, multifamily community on the subject property is in compliance with the applicable provisions of Subtitle 27 of the Prince George’s County Code (the “**Zoning Ordinance**”), *Prince George’s Plaza Transit District Development Plan and Transit District Overlay Zoning Map Amendment* (the “**TDDP**”), and other applicable review requirements and criteria. The subject property consists of approximately 2.42 acres located at the northwest quadrant of the intersection of America Boulevard and East-West Highway (the “**Property**”).¹ The Property is currently zoned M-U-I (Mixed Use – Infill) / T-D-O (Transit District Overlay) and is subject to the recommendations of the TDDP. The Property is located within the “Prince George’s Plaza Downtown” as designated by *Plan Prince George’s 2035 Approved General Plan* (the “**General Plan**”). Additionally, the Property is located within the municipal boundaries of the City of Hyattsville (the “**City**”).

As described in detail herein and shown on DSP-21006, the Applicant proposes a mixed-use community consisting of ±316 units of multifamily housing and approximately 2,052 square feet of ground floor commercial space for the Property (the “**Proposed Development**”).² The Proposed Development will complement existing and future development within the Downtown to foster a desirable transit-oriented mixed-use neighborhood and promote the Transit District as a regional destination. Accordingly, the Applicant respectfully requests approval of the subject DSP.

¹ The Property is identified as Parcel N in the “Prince George Center” subdivision, as recorded in the Land Records of Prince George’s County at Plat Book REP 205, page 43.

² The Applicant is submitting this DSP for approval of the Proposed Development. This DSP may be amended from time to time to accurately reflect modifications or changes to the Proposed Development on the Property.

II. PROPERTY DATA

<i>Location:</i>	Northwest Quadrant of the Intersection of America Boulevard and East-West Highway.
<i>Tax Map #:</i>	42-A2.
<i>Frontage:</i>	East-West Highway (MD 410) to the south. America Boulevard to the east. Belcrest Road to the west. Liberty Lane to the north.
<i>Election District:</i>	17.
<i>Legislative District:</i>	21.
<i>Councilmanic District:</i>	2.
<i>Acreage:</i>	±2.42 acres.
<i>Zoning:</i>	M-U-I (Mixed Use – Infill) T-D-O (Transit District Overlay)
<i>Planning Area:</i>	78.
<i>Subdivision:</i>	4-21006.
<i>Existing Water Category:</i>	W-3.
<i>Existing Sewer Category:</i>	S-3.
<i>Historic:</i>	N/A.
<i>Aviation Policy Area:</i>	N/A.
<i>Master Plan & SMA:</i>	<i>Prince George’s Plaza Transit District Development Plan and Transit District Overlay Zoning Map Amendment.</i>
<i>General Plan:</i>	<i>Plan Prince George’s 2035.</i>

III. LAND USE BACKGROUND

A. Plan Prince George's 2035 General Plan

The Property is located within the Prince George's Plaza "Downtown" as designated by the General Plan. The General Plan envisions the Downtown as a place for residents "to live in safe, walkable healthy communities that supply a variety of high-quality housing options – ranging in price, density, and type." The General Plan envisions the "revitalization of existing infrastructure and facilities to leverage public investment," and the strengthening of "older communities."

B. Prince George's Plaza Transit District Development Plan and Transit District Overlay Zoning Map Amendment

The Property is located within the boundaries of the TDDP – the guiding planning document for the Transit District Overlay Zone. The TDDP creates two distinct, but interconnected Character Areas – the Downtown Core and Neighborhood Edge – that capitalize on the Transit District's existing transit network, recreational amenities, and retail appeal. The Property is located within the Downtown Core Character Area of the TDDP, which is described as the Transit District's central activity hub, with a mix of residential, retail, and office development framing lively walkable streets.

The Transit District Overlay Zoning Map Amendment rezoned the Property from M-X-T (Mixed-Use Transportation Oriented) to M-U-I. Moreover, the TDDP's location-specific analysis of the Property's zoning change highlights significant underutilization of the Property considering its proximity to transit.

The resulting M-U-I/T-D-O zoning for the Property permits a broad range of uses in a variety of buildings, creating flexibility that is conducive to development and redevelopment. In order to encourage mixed-use, transit-supportive, walkable developments in the Downtown Core, both the Zoning Ordinance as well as Standards in the TDDP – particularly the Standard applying to "Bulk and Height, Density and Building Height," – stipulate that density for mixed-use residential/commercial

development in the M-U-I/T-D-O Zone is established at DSP.³ Accordingly, the density for the Proposed Development will be established by the subject DSP.

C. Conceptual Site Plan CSP-00024

The Prince George's County Planning Board (the "**Planning Board**") adopted Conceptual Site Plan CSP-00024 ("**CSP**") for the Property on November 9, 2000 and the Prince George's County District Council approved the CSP on January 8, 2001. The CSP was approved for the development of up to 2.245 FAR and 800-1200 dwelling units within University Town Center, a proposed mixed-use development with a "main street" theme that includes office, retail, and residential uses. The 38.62 acre area approved for development in CSP-00024 was divided into two Subareas – Subarea 2 and Subarea 3. The Property is approximately 2.42 acres located within Subarea 3.

The Property was rezoned to the M-U-I Zone, and retained in the T-D-O Zone, through the adoption of the 2016 Transit District Overlay Zoning Map Amendment. Pursuant to the general applicability and administrative section of the TDDP, a DSP in the Transit District does not have to conform to a previously approved CSP.⁴ Accordingly, CSP-00024, and the conditions of approval contained therein, are not relevant to the review of this DSP.

Additionally, the rezoning of the Property from M-X-T to M-U-I voided, in effect, the previously approved CSP.

D. Preliminary Plan of Subdivision 4-01092

On March 28, 2002, the Planning Board approved Preliminary Plan of Subdivision 4-01092 ("**PPS**"), for the development of residential and commercial uses on 25.12 acres, consistent with Subtitle 24 of the Prince George's County Code (the

³ Sec. 27-546.18(b) provides that "where an owner proposes a mix of residential and commercial uses on a single lot or parcel in the M-U-I Zone, the site plan as approved shall set out the regulations to be followed."

⁴ See page 195, Site Plan Review Requirement 3 "Pursuant to Section 27-548.08(c)(2), a Detailed Site Plan in a Transit District does not have to conform to a previously-approved Conceptual Site Plan. Accordingly, Conceptual Site Plans approved prior to July 19, 2016 have no bearing on the approval of a DSP for development in the Transit District."

“**Subdivision Ordinance**”). The Property was platted pursuant to PPS 4-01092 and is vested under PPS 4-01092 for transportation adequacy.⁵ However, previous development in University Town Center pursuant to PPS 4-01092 has fully utilized the recreational facilities provided by PPS 4-01092. Accordingly, these recreational facilities cannot apply towards the Proposed Development, and the Proposed Development will require additional recreational facilities. The Applicant has submitted PPS 4-21006 to address adequacy of recreational facilities for the Proposed Development.

IV. EXISTING AREA / NEIGHBORHOOD

The Property is located at the southern entrance of University Town Center, at the northwest quadrant of the intersection of East-West Highway and America Boulevard in the City. The Property fronts East-West Highway to the south, America Boulevard to the east, Liberty Lane to the north, and Belcrest Road to the west. Existing retail and residential uses within University Town Center are adjacent to the Property to the north and east. Future residential development in Metro I and Metro II are located directly to the north and east of the Property, across Liberty Lane. Retail, food store, and healthcare uses are located directly to the east across America Boulevard. The Property abuts an existing commercial office building to the southwest, on adjacent Parcel H, and existing retail at The Mall at Prince George’s is located to the west of the Property across Belcrest Road. Notably, the Property is in close proximity to mass transit, as the Prince George’s Plaza Metro Station is located approximately 800 feet southwest of the Property, across East-West Highway.

⁵ See Exhibit B, T. Masog email to N. Speech regarding transportation adequacy and trip cap analysis for the Proposed Development pursuant to PPS 4-01092.

IV. PROPOSED DEVELOPMENT

A. Overview

The Applicant proposes to transform existing surface parking at the Property into a contemporary, transit-oriented, mixed-use community that maximizes accessibility and connectivity for both future residents and patrons of the Downtown. This DSP involves the development of an eight-story, ±412,321 square foot mixed-use building, with ±308,690 square feet devoted to residential uses, and ±2,052 square feet devoted to ground-floor commercial uses. The Proposed Development offers a dynamic pedestrian-friendly exterior streetscape to activate the Downtown and complement existing commercial uses within University Town Center.

Additionally, the Proposed Development will provide ±12,812 square feet of interior amenity space. Programmatic areas will include: yoga and fitness spaces, communal work and lounge areas, a clubroom, and outdoor pool area. A large landscaped courtyard will be accessible to residents from the seventh floor. Active and passive recreation uses on the seventh floor will include a pool and other areas for outdoor grilling and seating. A multi-story parking structure will be constructed within the interior of the Property and wrapped by the mixed-use building, providing a total of 272 parking spaces for the Proposed Development. The location and strategic design of the garage on the interior of the Property contributes to a more pedestrian-friendly urban environment, offering improved circulation and consistent street walls along East-West Highway, America Boulevard, and Liberty Lane.

The Proposed Development will provide a total of ±316 dwelling units, with the following mixture of unit types: 75 studios (23.7% of the total units); 173 one-bedroom units (54.8%); and 68 two-bedroom units (21.5%). Units will be provided on the second, third, fourth, fifth, sixth, and seventh floors.

Overall, the Proposed Development promises a dynamic mixed-use community that complements existing uses within University Town Center, supports nearby mass transit, and serves as a visually stimulating landmark within the Downtown Core.

B. Architecture

Overall

The mixed-use building proposed for the Property addresses the varied topography of the site while complementing walkability and connectivity to University Town Center and the greater Transit District. This is achieved through activation of the ground floor through street level uses, pedestrian connections, and vehicular circulation to complement the adjacent uses.

East-West Highway & America Boulevard Corner

The proposed design at the corner of East-West Highway & America Boulevard envisions a gateway for the southern entrance of University Town Center to address the significance of this intersection to the built pedestrian environment at East-West Highway. This is accomplished by designing the main entrance and residential amenity at this location of the proposed building, connecting to the existing sidewalks, and emphasizing the entrance through various architectural design strategies. These strategies include the articulation of building forms, the simple use of materials, increased transparency at the corner, and breaking down scale and proportions of the building. The design also embraces the natural slope of the site, which features a low point along East-West Highway at its intersection with America Boulevard, revealing the entrance with extensive corner glazing extending along America Boulevard and East-West Highway. The corner entrance is also highlighted with a canopy and local mural artwork. The building features and materials are also highlighted at the corner using a unique balcony on the upper floors and the transitioning of exterior materials to articulate the corner.

Liberty Lane & America Boulevard Corner

The Liberty Lane & America Boulevard corner connects the proposed building to University Town Center by completing the retail corner at this intersection. The retail space features extensive glazing facing America Boulevard with the eight-story masonry façade extending along America Boulevard and continuing along Liberty

Lane at the stepped corner. An outdoor plaza for retail seating is created on the north side of the corner and also provides an opportunity for a mural created by a local artist.

C. Proposed Public Use Space, Streetscape, and Landscape Design

The Proposed Development will provide an impactful complement to the existing retail development across America Boulevard. This key site bookends the University Town Center central spine and acts as a gateway element. The proposed landscape and architecture will work in tandem to provide a rich palette of materials, textures and visual interest to be both memorable and unique. The building entrance and amenity spaces are located along the corner of America Boulevard and East-West Highway to activate that area of the streetscape and provide interest along this prominent frontage. Additionally, residential units will face the remaining area of the frontage, providing eyes along the pedestrian environment at this location. The streetscape along East-West Highway will extend the existing streetscape elements, continuing the pedestrian-friendly environment to the west with compatible design elements and new street trees.

The Proposed Development's frontage along America Boulevard largely mimics the present streetscape and provides street-level activation with the lobby, building amenity spaces, and retail that engage the streetscape at various points. Proposed retail at the corner of East-West Highway and America Boulevard will activate the streetscape with an outdoor seating terrace. The building edges frame the urban streetscape and are designed to adhere to and follow the TDDP guidelines.

VI. ANALYSIS

A. Compliance with Zoning Ordinance

1. §27-546.19 – Site Plans for Mixed Uses.

(c) A Detailed Site Plan may not be approved unless the owner shows:

(1) The site plan meets all approval requirements in Part 3, Division 9;

Comment: As discussed in Section VI(A)(3) of this Statement, the site plan meets all applicable approval requirements in Part 3, Division 9.

(2) All proposed uses meet applicable development standards approved with the Master Plan, Sector Plan, Transit District Development Plan, or other applicable plan;

Comment: This DSP meets all applicable development standards and/or represents a reasonable alternative for satisfying the site design guidelines. Alternative Standards are proposed herein and detailed in attached Exhibit A.

(3) Proposed uses on the property will be compatible with one another;

Comment: The proposed residential and commercial uses on the Property will be compatible with each other. The proposed multifamily and ±2,052 square feet of retail uses are both permitted at this location and, accordingly, are compatible with each other. Additionally, the proposed commercial uses will support both multifamily residential uses and the general public, providing a synergistic mixture of uses desired at this prominent location within the Transit District.

(4) Proposed uses will be compatible with existing or approved future development on adjacent properties and an applicable Transit or Development District; and

Comment: The proposed commercial and multifamily residential uses are compatible with existing and proposed adjacent development, which include medium- and high-density residential uses to the north and high-density commercial development to the north and east of the Property. The multifamily uses on-site will provide a residential base to support existing and future commercial uses on adjacent properties. Overall, the Proposed Development will enhance the surrounding mixed-use community within the Transit District's Downtown Core.

(5) Compatibility standards and practices set forth below will be followed, or the owner shows why they should not be applied;

(A) Proposed buildings should be compatible in size, height, and massing to buildings on adjacent properties;

Comment: The proposed eight-story mixed-use building is compatible in size, height, and massing of medium- to high-density residential and commercial buildings on adjacent properties. University Town Center is comprised of a series of different building heights and massing. Due to the varying grades on-site, the proposed building height varies. Measured from actual grade to parapet, the building's height varies from 73 feet to 90 feet. These heights are consistent with the heights of the adjacent buildings, including the nine-story building to the north on Liberty Lane and the five-story, approximately 80' tall office building at the corner of Belcrest Road and East-West Highway. The building to the north is very similar in size and massing to the proposed building.

(B) Primary facades and entries should face adjacent streets or public walkways and be connected by on-site walkways, so pedestrians may avoid crossing parking lots and driveways;

Comment: Primary facades and entries for the Proposed Development face East-West Highway and America Boulevard, two TDDP designated A Streets. Entries along both East-West Highway and America Boulevard connect the proposed mixed-use building to the surrounding community and adjacent residential and commercial uses located within University Town Center. Paved cross-walks designate connection points to adjacent commercial uses directly to the east of the Proposed Development. Structured parking on-site is accessed via B Street Liberty Lane and is designed to facilitate safe and unobstructed pedestrian circulation along the proposed building's entrances on East-West Highway and America Boulevard.

(C) Site design should minimize glare, light, and other visual intrusions into and impacts on yards, open areas, and building facades on adjacent properties;

Comment: The proposed mixed-use building will feature a rhythm of windows with appropriate glazing. Lights will illuminate the street, interior and exterior open spaces, and will have minimal impact on the adjacent properties.

(D) Building materials and color should be similar to materials and color on adjacent properties and in the surrounding neighborhoods, or building design should incorporate scaling, architectural detailing, or similar techniques to enhance compatibility;

Comment: The proposed building consists of masonry and cementitious siding in vertical and horizontal configurations. The colors include reddish-orange and black bricks with black and silver grey paneling. These materials and colors are consistent with and complementary to other buildings throughout University Town Center and adjacent neighborhoods. The building forms and scale are broken down through the use of articulated and repetitive building forms, window opening sizes and patterns, and detailing of the brick and paneling at the ground level and the upper levels.

(E) Outdoor storage areas and mechanical equipment should be located and screened to minimize visibility from adjacent properties and public streets;

Comment: Outdoor storage areas and mechanical equipment included with the Proposed Development will be appropriately screened or located strategically to minimize visibility from adjacent properties, public streets, and pedestrians.

(F) Signs should conform to applicable Development District Standards or to those in Part 12, unless the owner shows that its proposed signage program meets the goals and objectives in applicable plans; and

Comment: The Proposed Development conforms to the applicable TDDP signage Standards.

(G) The owner or operator should minimize adverse impacts on adjacent properties and the surrounding neighborhood by appropriate setting of:

- (i) Hours of operation or deliveries;*
- (ii) Location of activities with potential adverse impacts;*
- (iii) Location and use of trash receptacles;*
- (iv) Location of loading and delivery spaces.*
- (v) Light intensity and hours of illumination; and*
- (vi) Location and use of outdoor vending machines.*

Comment: The TDDP has multiple compatibility Standards and guidelines regarding building placement, orientation, design, lighting, outdoor storage, and signage. As discussed in Exhibit A, the Proposed Development includes appropriate settings for each of the above listed items and is compatible with applicable TDDP Standards.

2. §27-548.08 – Site Plan.

(c) Required findings.

(1) In addition to the findings required by Section 27-276(b) for approval of a Conceptual Site Plan in the T-D-O Zone, the Planning Board shall find that the Transit District Site Plan is consistent with, and reflects the guidelines and criteria for development contained in the Transit District Development Plan.

Comment: The mixed-use, multifamily community proposed in this DSP is consistent with the TDDP’s overall purpose to create “a walkable, transit-oriented community within the Prince George’s Plaza Transit District using a market-driven approach.” The following guidelines and criteria for development support approval of this DSP:

Land Use

The DSP furthers the following Land Use policies:

- *LU1: Promote a pedestrian-friendly, transit-supportive development pattern in the Transit District.*

- *LU2: Create sufficient residential capacity to help meet the County’s Growth Management Goal of 50 percent of new dwelling units within Regional Transit Districts.*
- *LU4: Concentrate medium- to high-density development in the Downtown Core.*

Comment: The Proposed Development will provide ±316 units of high-quality housing and ground floor commercial uses in close proximity to the Prince George’s Plaza Metro Station. In addition to activating the streetscape with engaging commercial space, this DSP will increase the residential density and housing capacity of the Downtown Core to advance the County’s Growth Management Goal of locating 50 percent of the County’s new dwelling units within Regional Transit Districts. Moreover, additional housing at this location will support and complement the Transit District’s existing amenities, retail, and commercial uses to foster a pedestrian-friendly, transit-supportive, and accessible Downtown. Overall, the Proposed Development offers high-quality transit-oriented development that advances the County’s development pattern and Growth Management Goals.

Economic Prosperity

This DSP furthers the following Economic Prosperity policies:

- *EP4: Diversify residential options to appeal to a range of current and future buyers and renters, including young professionals, first-time homebuyers, and seniors looking to age-in-place.*
- *EP5: Rebrand the Transit District and market it as an exciting, dynamic, and growing regional destination.*

Comment: With the addition of ±316 multifamily residential units, the proposed community expands and diversifies the range of residential options available within the Transit District. This DSP ensures that these new residential options are attractive and responsive to current market demand for a broad spectrum of prospective residents and households. Specifically, the Proposed Development will be particularly attractive to young professionals who value immediate proximity to mass

transit and amenities within the Transit District. Further, the high-quality residential product proposed in this DSP will help facilitate the rebranding of the Transit District as a dynamic emerging regional destination.

Transportation and Mobility

This DSP furthers the following Transportation and Mobility policies:

- *TM1: Incorporate street planning and design practices that allow Complete and Green Streets to facilitate the efficient movement of people throughout the Transit District while simultaneously serving as great public spaces.*
- *TM9: Maximize utility and usage of the pedestrian bridge over MD 410 (East-West Highway).*
- *TM11: Improve bicycle and pedestrian access to the Metro station.*
- *TM17: Manage the supply of parking within the Transit District to provide convenient, safe, and affordable parking options.*
- *TM19: As surface parking lots are redeveloped, provide all new off-street parking underground or in structures.*

Comment: The TDDP envisions “a multimodal network [that] will provide residents, employees, shoppers, and visitors a variety of safe, easy to navigate, and inviting ways to access destinations within and outside the Transit District.” The Proposed Development and resulting mixed-use multifamily community will activate underutilized mass transit and maximize the utility of multimodal linkages to transit. Additionally, the Applicant will provide bicycle and pedestrian improvements that promote walkability and increased multimodal connectivity, in accordance with the applicable Preliminary Plan of Subdivision. These improvements will facilitate safe and efficient movement for future residents and visitors as they travel within the Downtown Core and greater Transit District. Moreover, these new improvements – in connection with the existing pedestrian bridge over East-West Highway – will link the Proposed Development to Metro and destinations throughout the County and Region. Finally, the existing surface parking on-site will be redeveloped and replaced

with structured parking that is convenient, safe, and appropriately designed for this prominent location within the Downtown Core. These structured parking spaces will be unbundled from leases and rents associated with the Proposed Development.

Natural Environment

This DSP furthers the following Natural Environment policies:

- *NE4: Encourage the integration of green building techniques into all building designs to help reduce overall energy and water consumption.*

Comment: The TDDP envisions a “built environment that preserves and complements the natural environment in which it is located” and ultimately establishes a “sustainable and resilient community.” This DSP includes several green building techniques that meet this overall vision and advance the TDDP’s Natural Environment policies. A micro-bioretenion planter box and a subsurface structural facility are proposed to provide adequate storm water management on-site. Additionally, the Applicant is pursuing a minimum of an NGBS Bronze certification, which includes the use of recyclable, durable, and resource efficient materials while also reducing water consumption and overall energy use. These features will create a mixed-use community that advances environmental compatibility, addresses current issues, and mitigates future adverse impacts in advance.

Additionally, the brand of urban infill development proposed in this DSP represents one of the most effective sustainable development strategies available to Prince George’s County in terms of reducing overall energy, water, and resource consumption. The Proposed Development locates high-density residential uses within immediate proximity of commercial uses, public amenities, and mass transit. This transit-oriented, synergistic mixing of land uses promotes walkability and effectively reduces the number and duration of automobile trips to and from the Property to shrink the Transit District’s carbon footprint. Moreover, the urban infill development proposed in this DSP offers sustainable land use that efficiently utilizes the County’s resources, rather than inordinately stressing them with low-density residential

sprawl. The redevelopment of existing surface parking and provision of ±316 multifamily units in this DSP presents a marked step towards green and efficient land use within the Transit District that promotes the TDDP's and County's sustainability goals.

Housing and Neighborhoods

This DSP furthers the following Housing and Neighborhood policies:

- *HN1: Provide a variety of housing types and unit sizes, and neighborhoods to accommodate and meet the demands of existing and future residents in the Transit District.*
- *HN3: Minimize and mitigate adverse impacts of new and infill development on surrounding communities.*
- *HN4: Develop and promote a brand for the Transit District neighborhoods that highlights their unique features and enhances the Transit District's competitiveness within the regional housing market.*
- *HN5: Promote sustainable, green neighborhoods and housing options.*

Comment: The Housing and Neighborhoods element of the TDDP envisions a “diverse community of housing opportunities and neighborhood characteristics” that meets housing preferences and will stimulate mixed-use development in the Transit District. The subject DSP represents a major stride towards realizing this vision, as the ±316 residential units and ground floor commercial uses will provide additional housing opportunities and further cultivate a 10-Minute Neighborhood within the Downtown Core and overall Transit District.⁶ Moreover, this DSP represents appropriate infill development that welcomes hundreds of new households to activate underutilized commercial and transportation assets within the TDDP and complement existing and future development within the Downtown Core.

⁶ A 10-Minute Neighborhood is a community where residents can walk short distances from home to destinations that meet their daily needs, such as commercial, recreational, educational, and transportation uses. 10 Minutes represents the time it takes a typical pedestrian to walk ½ mile.

Accordingly, high-quality multifamily housing at this location will promote the Downtown's unique brand of accessibility and modern urban lifestyle to advance the Transit District as a competitive destination within the regional housing market.

Community Heritage, Culture, and Design

This DSP furthers the following Community Heritage, Culture, and Design policies:

- *HD1: Redevelop the Transit District at a walkable scale, with a network of Complete and Green Streets as its backbone.*
- *HD3: Redevelop the Transit District to the urban scale appropriate for a designated Regional Transit District.*
- *HD4: Create an active, interesting, enlivening, and dynamic public realm.*
- *HD5: Create significant urban design features at signature sites that establish a distinct identity of place, create symbolic gateways and significant points of interest, and contribute to the visual and architectural character of the Transit District.*
- *HD6: Create opportunities for artistic and cultural expression and events.*
- *HD7: Encourage a wide variety of modern signs, facilitating opportunities for property owners to utilize signage as an artistic expression as well as appropriate marketing and wayfinding.*
- *HD8: Limit regulation of architecture, encourage property owners to respond to market demands with visually appealing and cutting-edge architectural amenities.*

Comment: The Community Heritage, Culture, and Design element of the TDDP envisions “a memorable, walkable, vibrant, and welcoming regional urban destination.” With contemporary architecture and cutting-edge design elements, the proposed mixed-use multifamily community is responsive to market demands and aligns with the type of development encouraged by this vision. The development of this parcel will complete the gateway treatment into this area of the Transit District, as envisioned by the TDDP. The Proposed Development creates a pedestrian-friendly and exciting urban environment where residents and visitors can coningle and

interact. The streetscape continues the palette and design vocabulary present in the surrounding developed parcels, while appropriately addressing the TDDP standards. The architecture complements the area while also creating a unique building that emphasizes the gateway. Opportunities for murals by local artists are envisioned on the building façade. Additionally, dynamic and functional signage will be integrated into the overall community to create additional opportunities for place-based branding in the Transit District. In concert, the proposed heritage, culture, and design elements will foster a prominent and visually stimulating landmark at the southern entrance of University Town Center, welcoming residents and visitors from the Prince George's Plaza Metro Station and East-West Highway.

Overall, the Proposed Development will provide cutting-edge design features and amenities to cultivate a unique urban character appropriate for the Downtown Core, encourage visual interest at a signature location across from Metro, and advance the Transit District's placemaking and branding initiatives.

Healthy Communities

This DSP furthers the following Healthy Communities policies:

- *HC1: Ensure that permitted land uses benefit the overall health and wellness of the community.*
- *HC2: Incorporate a health and wellness focus in the desired network of urban and natural park spaces.*

Comment: This DSP will encourage a healthy lifestyle by promoting walkability through multiple opportunities for pedestrian activity and community interaction, as well as the provision of recreational facilities. This new addition to the urban fabric will create an environment that fosters social interaction, not only along the pedestrian-friendly streets, but also internally serving the residents. A lush and carefully crafted courtyard will provide opportunities for outdoor recreation and interaction. Areas for grilling, socializing and lawn games will provide much needed amenities along the exterior of the building. Additionally, a rooftop terrace featuring

a swimming pool, sun deck, plantings, additional grilling stations, and social areas will provide a new exterior amenity and socially engaging space that overlooks the at grade courtyard below. These amenities will promote health and wellness for future residents of the Proposed Development and greater Transit District.

Parks & Recreation

This DSP furthers the following Parks & Recreation policies:

- *PR1: Incorporate and design parks, open spaces, and recreational facilities throughout the Transit District to support health and wellness, create opportunities for social gatherings and public events, and foster a sense of place.*
- *PR3: Ensure the Transit District's park and open space network is connected to the pedestrian and bicyclist network emphasizing convenience, accessibility, and safety.*

Comment: The Applicant will provide high-quality on-site facilities for future residents. Wider sidewalks, as required by the TDDP, and other elements established by the applicable PPS will also be provided to promote connectivity, accessibility, and safety within the Transit District.

(2) The findings required by Section 27-285(b) shall not apply to the T-D-O Zone. Instead, the following findings shall be made by the Planning Board when approving a Detailed Site Plan in the T-D-O Zone:

(A) The Transit District Site Plan is in strict conformance with any mandatory requirements of the Transit District Development Plan;

Comment: See Exhibit A for analysis of the Transit District Standards.

(B) The Transit District Site Plan is consistent with, and reflects the guidelines and criteria for development contained in, the Transit District Development Plan;

Comment: As previously discussed, this DSP advances several policies under the TDDP's Land Use, Economic Prosperity, Transportation and Mobility, Natural Environment, Housing and Neighborhoods, Community Heritage, Culture, and Design, Healthy Communities, and Parks and Recreation Elements. Accordingly, this DSP is consistent with, and reflects the guidelines and criteria for development contained in, the Transit District Development Plan.

(C) The Transit District Site Plan meets all of the requirements of the Transit District Overlay Zone, and applicable regulations of the underlying zones, unless an amendment to the applicable requirement or regulation has been approved;

Comment: The Applicant incorporates its response in Section V. (A) (1) and Exhibit A.

(D) The location, size, and design of buildings, signs, other structures, open spaces, landscaping, pedestrian and vehicular circulation systems, and parking and loading areas maximize safety and efficiency, and are adequate to meet the purposes of the Transit District Overlay Zone;

Comment: The location, size, and architectural design proposed in this DSP are adequate to meet the purposes of the Transit District Overlay Zone, which “is intended to ensure that the development of land in the vicinity of Metro stations maximizes transit ridership, serves the economic and social goals of the area, and takes advantage of the unique development opportunities that mass transit provides.” Moreover, this DSP satisfies many of the specific purposes of the Transit District Overlay Zone, including, “to insure that developments within the Transit District possess a desirable urban design relationship with one another, the Metro Station, and adjoining areas.” The subject DSP achieves this goal by incorporating design elements that will foster a visually stimulating, transit-oriented mixed-use community that is desired for the Transit District’s Downtown Core.

(E) Each structure and use, in the manner proposed, is compatible with other structures and uses in the Transit District, and with existing and proposed adjacent development; and

Comment: The development of ±316 multifamily units and commercial space is compatible with the M-U-I/T-D-O Zone and surrounding structures and uses in the Transit District. This DSP promises a residential base to support existing retail, dining, and various commercial uses within the Transit District. Moreover, the proposed architecture and design elements represent compatible high-quality infill development that complements existing structures at the southern gateway to University Town Center.

(F) Requests for reductions from the total minimum required parking spaces for Transit District Overlay Zones pursuant to Section 27-548.09.02 meet the stated location criteria and are accompanied by a signed Memorandum of Understanding between a car sharing corporation or company and the applicant.

Comment: There is no required minimum number or ratio of off-street parking spaces in the TDDP.

(3) The applicant may ask the Planning Board to apply development standards which differ from mandatory requirements in the Transit District Development Plan, unless the plan provides otherwise. The Board may amend any mandatory requirements except building height restrictions and parking standards, requirements which may be amended by the District Council under procedures in Part 10A, Division 1. The Board may amend parking provisions concerning the dimensions, layout, or design of parking spaces or parking lots.

Comment: Please see Exhibit “A” for analysis of the Transit District Standards.

In approving the Transit District Site Plan, the Planning Board shall find that the mandatory requirements, as amended, will benefit the proposed development and the Transit District and will not substantially impair implementation of the Transit District

Development Plan, and the Board shall then find that the site plan meets all mandatory requirements which apply.

Comment: The mandatory requirements, as amended, will benefit the proposed development and the Transit District and will not substantially impair implementation of the Transit District Development Plan.

3. §27-285 – Planning Board Procedures.

(b) Required Findings.

(1) The Planning Board may approve a Detailed Site Plan if it finds that the plan represents a reasonable alternative for satisfying the site design guidelines, without requiring unreasonable costs and without detracting substantially from the utility of the proposed development for its intended use.

Comment: The subject DSP represents the most reasonable alternative for satisfying the site design guidelines without requiring unreasonable costs, and it does not detract substantially from the utility of the proposed development for its intended use.

(2) The Planning Board shall also find that the Detailed Site Plan is in general conformance with the approved Conceptual Site Plan (if one was required).

Comment: The Property was rezoned to the M-U-I Zone, and retained in the T-D-O Zone, through the adoption of the 2016 Transit District Overlay Zoning Map Amendment. Pursuant to the general applicability and administrative section of the TDDP, a DSP in the Transit District does not have to conform to a previously approved CSP. Accordingly, CSP-00024, and the conditions of approval contained therein, are not relevant to the review of this DSP.

(3) The Planning Board may approve a Detailed Site Plan for Infrastructure if it finds that the plan satisfies the site design guidelines as contained in Section 27-274, prevents offsite property damage, and prevents environmental degradation to safeguard the

public's health, safety, welfare, and economic well-being for grading, reforestation, woodland conservation, drainage, erosion, and pollution discharge.

Comment: This finding is not applicable to the subject DSP because this is not a Detailed Site Plan for Infrastructure.

(4) The Planning Board may approve a Detailed Site Plan if it finds that the regulated environmental features have been preserved and/or restored in a natural state to the fullest extent possible in accordance with the requirement of Subtitle 24-130(b)(5).

Comment: The Proposed Development does not impact any Regulated Environmental Features as defined by Section 24-101(b)(27) of the Prince George's County Code.

VII. MASTER PLAN TDDP & TDOZ MANDATORY STANDARDS

Please see Exhibit "A" for analysis of the Transit District Standards.

VIII. CONFORMANCE WITH PREVIOUS APPROVALS & OTHER APPLICABLE REVIEW CRITERIA

A. CSP-00024

The Planning Board adopted the Resolution of Approval for CSP-00024 on November 9, 2000. The District Council approved CSP-00024 on January 8, 2001. As previously mentioned, the Property was rezoned to the M-U-I Zone, and retained in the T-D-O Zone, through the adoption of the 2016 Transit District Overlay Zoning Map Amendment. CSPs are not required for development in the M-U-I Zone. Additionally, pursuant to the general applicability and administrative section of the TDDP, CSP-00024 – and the conditions of approval contained therein – are not applicable to the review of this DSP.⁷

⁷ See TDDP, page 195, General Applicability and Administration: Site Plan Review, SP3. "Pursuant to Section 27-548.08(c)(2), a Detailed Site Plan in a Transit District Does not have to conform to a previously-approved Conceptual Site Plan. Accordingly, Conceptual Site Plans approved prior to July 19, 2016 have no bearing on the approval of a DSP for development in the Transit District. Regardless of previous Conceptual Site Plan approval, all DSPs shall strictly conform to the current Transit District Standards.

B. PPS 4-01092

On March 28, 2002, the Planning Board approved PPS 4-01092 for the development of residential and commercial uses in University Town Center. As previously discussed, the Property is vested under PPS 4-01092 for transportation adequacy. However, the Proposed Development will require additional recreational facilities. Accordingly, the Applicant has submitted a new Preliminary Plan to address adequacy of recreational facilities for the Proposed Development.

C. PRINCE GEORGE'S COUNTY LANDSCAPE MANUAL

This DSP is subject to the Transit District Standards provided in the TDDP. The TDDP Standards replace the comparable Standards in the 2010 *Prince George's County Landscape Manual* (the "**Landscape Manual**"). For Standards not covered in the TDDP, the Landscape Manual shall serve as the requirement, unless explicitly stated otherwise in the TDDP. Analysis of the Applicant's compliance with the landscape requirements of the TDDP and applicable Landscape Manual requirements is contained in Exhibit A.

D. TREE CANOPY COVERAGE ORDINANCE

Pursuant to Sec. 25-127(b)(1)(I) of the Prince George's County Code, the Proposed Development is exempt from the Prince George's County Tree Canopy Coverage Ordinance because it is located within the boundaries of the TDDP.⁸ Accordingly, this DSP is regulated by applicable TDDP Standards concerning street, on-site, and other trees. Compliance with these Standards is detailed in Exhibit A.

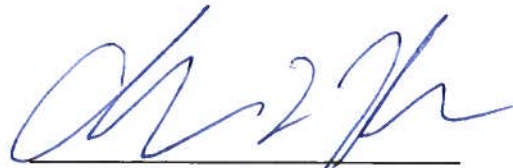
⁸ Sec. 25-127(b)(1)(I) of the Prince George's County Code states that "properties subject to tree canopy coverage requirements contained in an approved T-D-O Zone or a Development District Overlay Zone are exempt from the tree canopy coverage requirements contained in this Division." Pursuant to the TDDP, the tree canopy coverage requirements for the T-D-O Zone shall be met through the provision of street, on-site, and other trees preserved by a property owner or provided to comply with other Transit District Standards and guidelines.

IX. CONCLUSION

The Applicant respectfully requests the approval of DSP-21006 for the Proposed Development. As discussed throughout this Statement, the Proposed Development meets a demand for high-quality, transit-oriented residential development within the TDDP. In sum, the subject DSP presents an important opportunity for the County and the City to leverage existing transit infrastructure, activate the Downtown Core, and make progress towards the County's Growth Policy. The above analysis and submitted plans establish that the subject DSP satisfies the required findings that the Planning Board must make to approve a DSP application in accordance with the Zoning Ordinance.

Respectfully submitted,
LERCH, EARLY & BREWER, CHARTERED

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EXHIBIT A

Exhibit A
Transit District Standards Conformance
Downtown Core Character Area

The following analyzes the conformance of the Proposed Development to the Transit District Standards beginning on Page 201 of *The Approved Prince George's Plaza Transit District Development Plan and Transit District Overlay Zoning Map Amendment* (the "TDDP"). The prefatory provisions, including those addressing General Applicability & Administration and Exemptions, are noted but not discussed in detail.

Transit District Standards / Purpose

The purpose of the Transit District Standards is to shape the character of the built environment and fulfill the recommendations of the Prince George's Plaza Transit District Development Plan to create a walkable, mixed-use downtown. An essential component of any downtown is the public realm—streets, sidewalks, squares, and other open spaces—that creates the spine of a successful downtown, fosters community activity and provides a distinct sense of place (identity).

Comment: The Proposed Development was planned to conform closely to the Transit District Standards in order to create a walkable, transit-oriented, mixed-use community in the Downtown Core character area of the TDDP. Beyond the transformation of multiple acres of existing surface parking into a high-quality new urbanist development featuring ±316 multifamily units and ±2,052 square feet of ground floor commercial uses, the Proposed Development will serve as a new destination within the Downtown and advance the TDDP's placemaking goals to foster community activity at this prominent location.

Transit District Standards / Organization

The Transit District Standards are organized into three sections. The General Standards cover the entirety of the Transit District and govern all new development, and redevelopment located within its boundaries. They regulate the creation of a public realm, including streets, frontage zones, block size, building form, parking, and architecture. The Transit District consists of two

distinct character areas: the Downtown Core and the Neighborhood Edge. Unique standards are provided for each. The General Standards should be consulted first, followed by those for the Downtown Core or Neighborhood Edge

NOTE

The Transit District Standards consist of tables, cross-sections, and text. Graphics and tables containing standards are clearly labeled as “Standards.” Information found within tables labeled “Standards” represent mandatory requirements.

Dimensions identified in cross-sections of private streets to be constructed outside of the City of Hyattsville represent mandatory requirements. Dimensions identified in cross-sections of existing and proposed state, County, and municipal streets are illustrative in nature.

Comment: As noted above, the proposed development is located in the TDDP’s Downtown Core character area. The applicable general standards are each discussed below, as are the Downtown Core-specific standards.

Streets and Frontage / Streets

Standards

A street hierarchy is established between primary and secondary streets. Building form, entries, and service functions are tailored to the specific role of each. All new and existing streets shall be classified as one of the following, in order of importance:

A Streets

B Streets

Pedestrian Streets

Promenades

Alleys

All new A and B Streets constructed within the City of Hyattsville shall be constructed to the City of Hyattsville’s Street Design Standards.

All other new streets shall be constructed to the Transit District Standards; travel lanes may be constructed to less than 11 feet in

width only with the written permission of the Director of the Department of Permitting, Inspections and Enforcement.

Comment: The Proposed Development does not include any new streets.

Except where natural resource conservation, parkland, or vertical changes in grade that exceed the maximum grade permitted for construction of a road prevent connection to another street, forcing the creation of a dead-end, all streets shall terminate at other streets, forming a network.

Comment: The Proposed Development does not include any new streets, nor does it propose dead-end streets.

Alleys or service drives, where necessary, shall be included within blocks and shall not divide one block from another.

Comment: This Standard is noted. Maximum block lengths are calculated from street to street; alleys are not considered in the application of this Standard.

Medians shall be prohibited on all new private streets and are discouraged on all new public streets, except as needed to accommodate the Central Plaza recommended by Strategies TM3.2 and HD4.3

Comment: The Proposed Development does not include any new streets or medians.

Along private streets, crosswalks shall be provided at all intersections and shall use highly visible markings and/or decorative alternative paving material.

Along private streets, all signalized intersections with bike lanes, cycle tracks, or any type of bicycle facility as part of the road design shall include bike boxes that allow bicyclists to proceed on green ahead of motorized vehicles.

Comment: There are existing crosswalks at all intersections adjacent to the Proposed Development. There are no signalized intersections proposed.

Each end of all crosswalks within the Transit District shall have a dedicated curb ramp.

Comment: Curb ramps are proposed at the end of all crosswalks.

Guidelines

Internal streets should provide access or connection, wherever possible, to adjacent properties within the Transit District.

Comment: No internal streets are proposed for the Property.

Along public streets, crosswalks should be provided at all signalized intersections and should use highly visible markings and/or decorative alternative paving material.

Comment: There are no signalized intersections adjacent to the Proposed Development.

Decorative paving that clearly defines an intersection of private streets is permitted.

Comment: Decorative paving is not proposed within the private streets, as the adjacent private streets already include decorative paving.

Promenades and Pedestrian Streets should only be constructed in the Downtown Core.

Comment: The Proposed Development does not include the construction of Promenades and Pedestrian Streets.

Streets and Frontage / Blocks

Standards

The street network shall define blocks of up to 500 linear feet on each side. Each side of a block shall not be more than 500 linear feet from right-of-way to right-of-way.

Comment: This Standard is acknowledged. The length between America Boulevard and Liberty Lane is 459', the block along East-West Highway is 248', the block along America Boulevard is 267', and the block along Belcrest Road is 130'.

For all new redevelopment on existing block frontages of greater than 500 linear feet, a perpendicular A Street or B Street (or, in the Downtown Core, a Pedestrian Street) shall be constructed at no greater than a 500-foot interval from the nearest existing or proposed street.

Comment: This Standard is not applicable, as no existing block frontage is greater than 500 linear feet.

A block shall be divided by an A, B, or Pedestrian Street, or a Promenade, to allow a pedestrian or vehicular connection from the front of the block to the rear of the block.

Comment: Dividing streets include Liberty Lane and America Boulevard

Alleys or service drives, where necessary, shall be included within blocks and shall not divide one block from another.

Comment: This Standard is acknowledged.

Streets and Frontage / Frontage Zones

All existing and proposed A, B, and Pedestrian Streets shall have sidewalks on both sides constructed to the frontage standards prescribed in this plan. At a minimum, all sidewalks shall have a Sidewalk Clear Zone and a Tree and Furnishing Zone. Provision of Buffer Zones, Residential Frontage Zones, or Retail Zones is optional, as needed.

Comment: The Project's boundary consists of A and B streets. East-West Highway, Belcrest Road and America Boulevard are A streets and Liberty Lane is a B street. There is a 10' public utility easement (PUE) along East-West Highway and Belcrest Road. With the PUE, the project will have a frontage depth of 25' along East-West Highway, per the TDDP. Including the PUE, the frontage depth along Belcrest Road is 24' and still within the maximum build-to-line of the TDDP.

On America Boulevard, a build-to line of 15' is proposed to address and create an urban edge for this street and respond to the adjacent development on the east side of America Boulevard Accordingly, the Applicant requests a modification to provide less than the 20' frontage zone provided by the TDDP.

On Liberty Lane, a build-to line of 10' is proposed to allow for a viable development depth and create an urban edge for pedestrians on this street. The development landscape design focuses on a robust streetscape for the community and pedestrian experience. Accordingly, the Applicant requests a modification to provide less than the 15' frontage zone provided by the TDDP.

On America Boulevard (A Street), the proposed sidewalk zones are consistent with those of the surrounding developments on America Boulevard. These consist of:

- 4 ft. Tree and Furnishing Zone,
- 6 ft. Clear Sidewalk Zone and
- 5 ft. Buffer Zone.

On Liberty Lane (B Street), the sidewalk varies from the Standard due to the constrained streetscape. It consists of:

- 5 ft. Clear Sidewalk Zone at the back of curb
- 4.5 ft. Buffer Zone to residential units.

Both proposed build-to-line modifications were presented to Planning Staff during the December 4th, 2020 Pre-Application Meeting. Each modification was received favorably by Staff during this meeting.

Where Retail Zones are provided, they shall be located as follows:

- *Retail Zone 1: Adjacent to the building.*
- *Retail Zone 2: If necessary to meet eating and drinking establishment seating needs, adjacent to, or integrated with, the Tree and Furnishing Zone.*

Comment: Retail Zone 1 is proposed at the northeast corner of the building. On America Boulevard this consists of a 5' wide area adjacent to the retail use. On Liberty Lane it is an approximately 8' wide terrace between the building and existing transformer vaults.

Tables 42 and 43 contain dimensional standards for frontage zones and new private streets. The Total Frontage Depth Requirements referenced in the text and identified in Table 44 and Figures 9-24 (see pages 213-230) reflect the minimum amount of frontage required for each street. The maximum amount of frontage permitted is equivalent to this minimum, plus 5 feet in the Downtown Core and the minimum plus 10 feet in the Neighborhood Edge.

Individually, the Buffer Zone, Retail Zone, and Residential Zones have no dimensional requirements. Their dimensions will be governed by the amount of each zone needed to support the

proposed development and the total amount of required frontage. (See Tables 42 and 43 on pages 211-212.)

Comment: The Project's proposed frontage zones and compliance with the TDDP's Frontage Depth Requirements are detailed above in response to the Frontage Zone standard. The Applicant is requesting a modification from Frontage Depth Requirements along America Boulevard and Liberty Lane to provide an urban edge along both streets and respond to adjacent development confronting the Property.

On America Boulevard (A Street), the proposed sidewalk zones are consistent with those of the surrounding developments on America Boulevard. These consist of:

- 4 ft. Tree and Furnishing Zone,
- 6 ft. Clear Sidewalk Zone and
- 5 ft. Buffer Zone.

On Liberty Lane (B Street), the sidewalk varies from the Standard due to the constrained streetscape. It consists of:

- 5 ft. Clear Sidewalk Zone at the back of curb
- 4.5 ft. Buffer Zone to residential units.

The Sidewalk Clear Zones and Tree and Furnishing Zones shall be consistent along a block.

Comment: Proposed sidewalk zones are consistent along each block frontage.

Primary building entrances or exits shall not open directly into a parking lot, onto a driveway (where permitted), side street, alley, loading dock, or other vehicle cartway.

Comment: Primary building entrances and exits are located on A Streets East-West Highway and America Boulevard. Neither of these locations open directly into a parking lot, a driveway, side street, alley, loading dock, or other vehicle cartway.

On A Streets, B Streets, Pedestrian Streets, or Promenades, no new public utilities, including, but not limited to, transmission or distribution lines and mechanical equipment, are permitted above-ground.

Comment: There are no above grade utilities proposed with this project.

Streets and Frontage / Build-To Lines and Zones

Standards

The minimum BTL shall be established at the rear of the frontage zones (except for promenades).

The total frontage depth requirement in Tables 42- 43 and Figures 9-23 shall represent the distance between the street curb and BTL.

The maximum BTL shall be 5 feet further from the curb in the Downtown Core, and 10 feet further from the curb in the Neighborhood Edge, than the minimum BTL.

The area between the minimum and maximum BTL shall be the BTZ.

Comment: See above response to Frontage Zone Standards. The Project complies with the applicable Standard along East-West Highway and Belcrest Road. The Applicant is requesting amendments to Frontage Zone Standards along America Boulevard and Liberty Lane.

Streets and Frontage / Build-to Lines and Zones / Building Entrances

Standards

Primary entrances shall be located at the front façade of the buildings. If a building fronts on both A and B Streets, Pedestrian Street, or Promenade, its primary entry shall be located on the A Street.

Service entrances shall be at the rear of the building.

Comment: Consistent with the TDDP, primary building entrances and exits are located on America Boulevard (A Street). The loading dock and garage entrance are proposed along Liberty Lane (B Street).

Streets and Frontage / Build-to Lines and Zones / Articulation

Standards

Unless otherwise stated in the Transit District Standards, bays, balconies, cornices, and blade signs may only encroach across the build-to line for up to 25 percent of the building frontage.

For multifamily residential buildings, bays, balconies, cornices, and shading devices shall not encroach across the build-to line beyond 10 feet.

Porches and stoops may only encroach beyond the BTL within the Residential Frontage Zone for a maximum of nine feet.

To provide visual interest and to reduce the perceived massing and scale of buildings, building architecture shall incorporate at least three of the following design elements:

- *Colonnades*
- *Gables*
- *Hip roofs*
- *Towers*
- *Dormers*
- *Archways*
- *Covered entries*
- *Repetitive window openings*
- *Arched and corbelled parapets*
- *Cornices*
- *Transoms*
- *Other design elements deemed appropriate by the Planning Board*

Comment: The subject DSP complies with the above listed articulation Standards.

Guidelines

Architectural treatment is recommended at the expression line to provide a shadow line, pattern, or texture effect. The treatment may be molding, projection, material change, or architectural element that serves to separate the lower story from the upper stories of the building façade at that location.

Roof lines, parapets, and building heights should be architecturally articulated, diverse in design form, and varied in height with adequately scaled and proportioned architectural

cornices and applied treatments that complement the building's design.

Upper-story wall treatment should be coordinated and complement the design of the lower-story façade.

Along primary building façades facing streets, façade articulation should average a minimum of 50 feet.

Blank walls should be avoided because they disrupt the street pattern and make an uninviting pedestrian experience; however, if necessary, blank walls should not be more than 10 to 20 feet wide.

Building corners should be articulated.

Comment: The mixed-use building complies with these guidelines as outlined below. The building is designed with 2 motifs. The 2 motifs are visually tied together with a masonry base that extends from the first floor to the fourth floor slab. The masonry base consists of two face brick colors in order to further articulate the base. Face Brick 1 is a reddish-orange brick and Face Brick 2 is a black brick.

Motif A is provided along America Boulevard and the northeast corner of the building at the America Boulevard and Liberty Lane intersection. Motif A consists of masonry with fiber cement accents to the 8th floor with vertical or horizontal cementitious siding from the 8th floor to the parapet. The masonry façade is comprised of the 2 face brick colors, as previously discussed. The masonry facade defines the major façade element bay on Motif A. The three bays are approximately 45-65 feet in width. The 8th floor fiber cement is articulated with three colors and textures.

Motif B is provided along Liberty Lane, East-West Highway and at the main lobby entrance at the southeast corner of the building at East-West Highway and America Boulevard. Motif B consists of the masonry base to the 4th floor slab and fiber cement siding articulated with three colors and textures. Fiber Cement Color 1 is a medium gray with a horizontal siding texture. Color 2 is a darker gray that is used at both horizontal siding texture accents and as a vertical siding texture that is

continuous from the 1st floor to the parapet. This element is used at the recesses in the plan to break down the scale of façade, creating the bays and a rhythm of the major facade elements in both motifs. Color 3 is a silver gray that is used on fiber cement panels with varied vertical battens and defines the major façade element bay at Motif B. The three bays along East-West Highway are approximately 76' in length. The four bays along Liberty Lane are approximately 65-90' in length.

All corners of the building are articulated with glazing and façade treatments that wrap the corner. Additionally, the main entrance is located at the southeast corner along the intersection of East-West Highway and America Boulevard. This corner is the gateway to University Town Center and the architecture and use at this corner reflects its significance. The corner is provided with significant transparency and a high floor height at the lobby. The parapet height at the main entrance corner is higher than the other components to emphasize the entrance.

A retail space is located on the 1st floor at the northeast corner of the building at the America Boulevard and Liberty Lane intersection. This use completes the retail corner at this intersection, which features existing retail at the other three corners. Additionally, the corner retail storefront is articulated with a canopy, high ceiling heights and significant glazing to activate the retail and the sidewalk activity.

Since this is a multifamily building, most elements of the façade have windows for the residential units, limiting blank walls on the elevations. The only blank wall that exceeds 25' in length is facing Liberty Lane. It is approximately 30' in length and is envisioned as a wall for a local artist mural.

Streets and Frontage / Alleys

Alleys in the Downtown Core shall have a Sidewalk Clear Zone; the Sidewalk Clear Zone shall be a minimum of four feet wide and a maximum of eight feet wide.

In the Neighborhood Edge only, residential driveways are permitted on Alleys.

Comment: This Standard is not applicable, as no alleys are proposed in this DSP.

Streets and Frontage / Tree and Furnishing Zones

GENERAL

Guideline

All public or privately installed street furniture, including lights, benches, waste receptacles, mailboxes, newspaper boxes and bicycle racks, or similar elements should be consistent within a project and be placed at regular intervals within the Tree and Furnishing Zone.

Comment: Lighting, benches, trash, and recycling receptacles are located at regular intervals along the streetscape of America Boulevard, consistent with adjacent development.

STREET TREES

Standards

Street trees shall be located within the Tree and Furnishing Zone; additional trees may be provided within the Retail or Residential Zones, as appropriate.

Shade trees two and one-half to three-inch caliper in size, shall be planted along each street with spacing of not greater than 40 feet on center, excluding driveway openings. Spacing allowances may be made, where necessary, to accommodate curb cuts, fire hydrants, and other infrastructure elements.

Ornamental trees, seven to nine feet in height, may only be used to meet the requirements of this section where overhead wires prohibit the planting of shade trees.

Comment: Street trees are proposed along East-West Highway and America Boulevard, consistent with the spacing and installation size Standards.

Guidelines

Each street should use a consistent species pallet that supports the long-term health of the trees.

Street trees should be located a minimum 35 feet from the point of curvature of an intersection of two streets.

Street trees should be located a minimum 10 feet from the point of curvature of residential driveway entrances.

Street trees should be located a minimum 20 feet from the point of curvature of commercial driveway entrances.

Street trees should be located a minimum 15 feet from the center of street light poles.

Street trees should be located a minimum 10 feet from water meters.

Street trees should be located a minimum 10 feet from storm drain inlets, hydrants, or manholes.

Comment: Street trees are proposed along East-West Highway and America Boulevard, consistent with the spacing and size requirements indicated where possible. In order to maintain consistent tree spacing, one tree at the intersection of America Boulevard and Liberty Lane is closer to the intersection and a storm inlet than guidelines recommend. Liberty Lane does not have room for street trees while providing appropriate buffering to residential units.

TREE BOXES

Tree boxes are openings in the sidewalks for street trees.

Standard

Tree grates are prohibited.

Comment: Tree boxes and planting strips have been provided. No tree grates are proposed.

Guidelines

If tree boxes are provided, all boxes along the length of a single block should be of the same type for a uniform appearance and the underground soil volumes should be connected. Uniform type around the entire length of a street within the Transit District, or around the entire perimeter of a plaza, square, or pocket park is preferable.

Tree boxes or pits may include additional landscaping. In addition to the tree, the tree box or pit may include flowering plants and

shrubs. However, no plants with thorns or other sharp protrusions should be used, and these plants should be maintained below a height of 42 inches.

Tree box openings should be delineated from hard surfaces to prevent soil compaction.

Tree boxes adjacent to on-street parking areas should be spaced to provide adequate pedestrian access to other frontage zones.

Comment: Tree boxes are consistent in size and will provide additional plantings below a height of 42 inches. Continuous soil panels are proposed under the pavement between tree boxes.

Streets and Frontage / Street Lights

Standards

All pedestrian rights-of-way—including sidewalks, trails, paths, and pathways from building entrances and exits to the sidewalk—shall be continuously lit.

Comment: The proposed streetlights will ensure that all applicable pedestrian rights-of-way are continuously lit.

Street lights shall either be pedestrian-scale fixtures or a combination of a street light and a pedestrian fixture. Pedestrian lights shall be no higher than 14 feet. Existing street lights shall not be counted toward this requirement unless they include pedestrian-scale fixtures that meet this standard.

Street lights installed along MD 410 (East-West Highway) and Belcrest Road shall use PEPCO's Teardrop or equivalent style from PEPCO's most recent Street Light Catalog.

All other streets in the Transit District shall use PEPCO's Acorn or equivalent style from PEPCO's most recent Street Light Catalog.

Cobra fixtures and high pressure sodium fixtures shall not be permitted.

Comment: The proposed streetlights will match the adjacent development's streetlights to provide continuity and will utilize energy efficient LED technology.

Street light fixtures shall be spaced a maximum of 40 feet apart in the Downtown Core.

Street light fixtures shall be spaced a maximum of 50 feet apart in the Neighborhood Edge, and may be placed in a staggered arrangement.

Comment: The proposed public streetlights will be installed at regular intervals in accordance with the TDDP, as illustrated for reference on the Detailed Site Plan.

Guidelines

Street light fixtures should allow for the hanging of banners and other amenities, including artwork, hanging flower baskets, etc.

Street light fixtures should include electric weather protectant receptacles.

Energy-efficient lighting should be used to conserve energy and reduce long-term cost.

Comment: The proposed streetlights will align with Pepco's practices and will be energy-efficient LED light fixtures to conserve energy.

Bulk and Height / Intent

To create opportunities for walkable, mixed-use development that support the transformation of Prince George's Plaza into a Regional Transit District and reinforces the street and sidewalk as the focal points of activity.

Comment: The proposed development was planned to conform closely to the TDDP Standards in order to create a walkable, street-oriented residential community in the Downtown Core character area of the Plan – which will support existing and future mixed-use activities in the Regional Transit District.

Bulk and Height / Lots and Building Placement

Standards

Each new lot shall abut an A, B, or Pedestrian Street to permit, at a minimum, emergency vehicle access.

When a building abuts multiple classes of a street, it shall face and front onto the highest level of the street. For example, if a

building abuts an A Street and a B Street, that building shall front onto the A Street.

Comment: The building entrance is located at the corner of East-West Highway and America Boulevard, the two A Streets at the Property. The entrance faces America Boulevard.

Guidelines

Buildings should occupy the perimeter of a block.

Buildings constructed on corner lots should not extend beyond the façade of other buildings on the block, to minimize sight distance impact

Comment: The proposed building occupies the perimeter of each block.

Bulk and Height / Density and Building Height

These standards establish building height limits which will permit the density of development appropriate to a Downtown, and a context-sensitive transition to a scale compatible with surrounding neighborhoods. In keeping with best practices in transit-oriented development, the greatest height is established in the Downtown Core, along Belcrest Road and MD 410 (East-West Highway).

A story is defined as “an interior space measured from one finished floor to the next finished floor or roof above.”

Section 27-548.06(a)(1) establishes that development within a Transit District shall not exceed the specified maximum residential density and any floor area ratio (FAR) requirements of the underlying zones. For ease of reference, the relevant density maximums are listed in the following table.

Pursuant to Section 27-548.06(b), this TDDP establishes the height regulations for the Transit District. The height regulations for buildings consist of five features:

- 1. Single-story buildings.*
- 2. Minimum first (ground) floor height.*
- 3. Stepback and transition requirements (where applicable).*
- 4. Height Maximums*

5. *Public amenity height bonuses.*

Table, Page 235:

Zone Maximum Permitted Density

M-U-I (Residential/Commercial) Established by DSP

Comment: The Proposed Development will include ±316 multifamily units and approximately 2,052 square feet of commercial space on ±2.42 acres.

Bulk and Height / Density and Building Height / Maximum Building Heights

As a new Regional Transit District, Prince George's Plaza will attract new residents, visitors, and workers and help sustain the existing population. This will, in turn, drive demand for new, modern, easily accessible public facilities. Much of the Transit District and surrounding community is built out, and construction of new public facilities will require coordination with property owners and stakeholders; innovative approaches to design, function, and colocation of facilities; and incentives to encourage provision of public facilities within the Transit District.

GENERAL

Standards

For the purposes of this TDDP, building height shall be measured in number of stories above the ground-level public entrance at the highest elevation. Differences in topography may permit ground-level entrances at lower elevations; these stories shall be considered basement or subgrade floors and shall not count against the story maximum. For example, a building may front on side A at street level; an entrance on side B may be two floors below the entrance on side A. The height of the building would be measured in stories from side A.

When a mezzanine is present, the mezzanine shall be counted as an additional story.

Habitable space in roof and attic areas with a sloping roof shall not be counted as a story.

Any portion of the building below the ground level shall not count as a story.

Raised basements shall not exceed one-half of a story in height above grade along the build-to line.

Guideline

Corner towers, vertical bays, and other features should be utilized to give the appearance of a taller building height.

[Map 34. Maximum Building Heights]

16 Stories (Subject Property)

Comment: The maximum building height proposed is eight stories.

SINGLE-STORY BUILDING HEIGHT AND FRONTAGE

Standards

Single-story buildings and single-story portions of multistory buildings are prohibited on corner lots in the Downtown Core.

Single-story buildings in the Downtown Core, or portions of multistory buildings consisting of a single story, shall have a façade of at least 20 feet in height measured from the average mean building grade at the front of the building to the top of the façade, with a minimum floor-to-ceiling height of 14 feet.

Comment: The Proposed Development is located in the Downtown Core and does not contain any portions consisting of a single story.

Commercial, institutional, mixed-use, or multifamily residential buildings: the first (ground) floor shall be at least 20 feet high, with a floor to ceiling height of at least 14 feet.

Comment: The first floor of the mixed-use building is 10' high, with a floor to ceiling height of 9'+/- at the residential units. The design provides a minimum 14' high ceiling spaces at the residential lobby entrance and retail corner to provide an open streetscape experience. A lower ceiling height is proposed for the residential units.

No accessory building within the Neighborhood Edge shall be taller than two stories.

Comment: The Proposed Development is located in the Downtown Core.

The maximum height of all other buildings is regulated by the standards (on page 236).

Comment: The eight-story mixed-use building complies with the 16-story maximum height standard.

STEPBACKS AND TRANSITIONS

Standards

All buildings or portions of buildings, within 250 feet of the northern boundary of the Oliver Street right-of-way shall be a maximum of six stories.

Comment: No portions of the subject property are located within 250 feet of the northern boundary of the Oliver Street right-of-way.

All buildings or portions of buildings, within 500 feet of the western boundary of the Adelphi Road right-of-way shall be a maximum of four stories.

Comment: No portions of the subject property that are located within 500 feet of the western boundary of the Adelphi Road right-of-way.

Guidelines

All buildings should have step back of at least 10 feet above the fourth story and may have subsequent setbacks.

Comment: The building does not feature any setbacks.

On sites or lots with multiple buildings, buildings closer to MD 410 (East-West Highway) should be tallest, with building heights stepping down as they are sited closer to the adjacent community outside the Transit District. Buildings adjacent to the Transit District border should be the shortest buildings on a particular site or lot.

Comment: The Proposed Development features a single multi-story mixed-use building on Parcel N.

Bulk and Height / Density and Building Heights / Public Amenity Height Bonus Program

One way in which this TDDP can incentivize the construction of public facilities is through the provision of height bonuses. These bonuses effectively permit the construction of taller buildings in

exchange for the provision of land, buildings, or building space for public facilities.

No height bonus shall be granted in the T-D-O/R-O-S, T-D-O/O-S, T-D-O/R-80, T-D-O/R-20, or T-D-O/R-18 Zones.

Comment: The Proposed Development is not utilizing height bonuses. Thus, the remainder of the provisions of this section of the Transit District Standards are not reproduced here.

Site Elements / Intent

To create an integrated system of safe environmentally-conscious and inviting spaces that encourage and facilitate active and passive uses, social interactions, and cultural events easily accessible via a short walk.

Site Elements / Placemaking and Open Spaces

Standard

Privately owned and/or operated open spaces within the Downtown Core that abut a street or frontage zone shall be accessible to the public through a public use easement or other mechanism.

Comment: The proposed development does not include open spaces that abut a street or frontage zone.

Guidelines

Other privately owned and operated open spaces should be accessible to the public whenever feasible through a public use easement or other mechanism.

Comment: The proposed development does not include publicly accessible open spaces.

Open spaces should be designed in accordance with the Formula 2040 Park Typology and their associated characteristics.

Comment: The proposed development does not include publicly accessible open spaces.

Consideration should be given to providing open spaces for important events or programming including, but not limited to, fitness activities; passive and active uses; toddler and child urban playground; neighborhood and community parks; dog parks; open greens; and picnic areas.

Comment: Interior amenity spaces for the Proposed Development will include yoga and fitness spaces, communal work and lounge areas, a clubroom, and a landscaped courtyard with passive recreation areas and an outdoor pool area.

Permeable materials are encouraged, wherever possible, to facilitate the growth of trees and vegetation, and the absorption and treatment of rainwater runoff.

Comment: The landscaped courtyard will provide extensive pervious surfaces, including planted areas and a large turf area. Planters at the pool level are intended to also absorb water, despite being over a concrete structure.

Site Elements / Placemaking and Open Space

MURALS AND PUBLIC ART

Guidelines

Murals or works of public art are encouraged in the Transit District. Wherever possible, such displays should reflect the aesthetic and cultural traditions of Hyattsville and Prince George's County, including their past, present, and future, and the environmental and geographic characteristics that make both the City and the County unique places.

Works of art, architectural enhancements and special landscape treatments should be located in areas where residents and visitors live, work, or congregate and should be highly visible and accessible.

Comment: A local artist mural is envisioned along the Liberty Lane streetscape at the northeast corner of the Property. The proposed mural area is approximately 30' in length and will be highly visible to residents and visitors of University Town Center.

COLOR

Guideline

The use of bright colors for trim and accent elements is encouraged.

Comment: This Guideline is acknowledged.

Site Elements / Lighting

Guidelines

Exterior areas, rear entryways, public spaces, roads, crosswalks, sidewalks, pedestrian overpasses, and trails should be well-illuminated to ensure safety and improve visibility while minimizing light spillover to other properties.

Comment: Exterior lighting of the streets will be provided by streetlights in accordance with the TDDP Standards.

Light fixtures should be placed so as to avoid conflicts with trees or other obstructions and should direct light to specific locations and away from adjoining properties. Consideration of security and pedestrian comfort should be prioritized by increasing illumination low to the ground in public parking lots, at building entrances, in public plazas and open spaces, and transit stops.

Comment: Streetlights will be located in accordance with the applicable Standards to avoid conflicts with trees and other obstructions. Appropriate lighting that promotes security and pedestrian comfort will be provided at building entrances and in structured parking at the Property.

Site Elements / Green Infrastructure

Guidelines

To the maximum extent practicable, no private development should be permitted within the Primary Management Area.

Comment: No development is proposed within the Primary Management Area.

Environmentally sensitive stormwater management facilities customarily found in a city or urban area should be used where appropriate, including subsurface collection facilities under parking lots, to store and slowly infiltrate stormwater.

Comment: The Property's approved Stormwater Management Plan features one (1) micro-bioretention planter box and one (1) subsurface structural facility to ensure environmentally sensitive stormwater management on-site.

Site and street designs should avoid the use of unshielded roof, side, and parking lot lights, and include the use of full cut-off optic lighting systems that provide consistent lighting levels.

Comment: The proposed streetlights will match the adjacent development's streetlights to provide continuity. They will utilize energy efficient LED technology and will be spaced for consistent lighting.

Pervious paving is encouraged throughout the Transit District. Examples include:

- *Cast-pressed concrete paver block*
- *Grassed cellular plastic or concrete*
- *Stamped asphalt or concrete*
- *Pea gravel, washed stone, or paving blocks*
- *Wood or concrete paving block*

Comment: Streetscape paving is to match the existing adjacent streetscape for consistency. This streetscape is cast-in-place concrete with paver accents. The landscaped courtyard will provide extensive pervious surfaces, including planted areas and a large turf area. Planters at the pool level are intended to also absorb water, despite being over a concrete structure.

Stormwater management measures should be integrated with bioswales, naturalistic green fingers, rain gardens, waterscapes and other green and environmental site design practices that minimize stormwater runoff and increase the infiltration of rainwater into the ground.

Comment: The Property's approved Stormwater Management Plan aligns with ESD practices to minimize runoff and ensure environmentally sensitive stormwater management on-site.

New development should be encouraged to adopt LEED-ND or similar sustainability standards, incorporating innovative solutions such as green buildings, reuse of gray water, reusable

energy generation onsite, technologies that lower electricity consumption, as well as green and habitable roofs where appropriate (rooftops that building occupants can use for gardening, socializing, and sunning).

Comment: The project is pursuing a minimum of a NGBS Bronze certification, with a chapter dedicated for Resource Efficiency. This includes the use of recyclable, durable and resource efficient materials, as well as locally sourced materials

Site Elements / Tree Canopy Coverage

Standard

Section 25-127(b)(1)(I) of the County Code states that “properties subject to tree canopy coverage requirements contained in an approved Transit District Overlay (T-D-O) Zone or a Development District Overlay (D-D-O) Zone are exempt from the tree canopy coverage requirements contained in this Division.” Pursuant to this section, the tree canopy coverage requirements for the Prince George’s Plaza Transit District Overlay Zone shall be met through the provision of street, on-site, and other trees preserved by a property owner or provided to comply with other Transit District Standards and guidelines.

Comment: The Proposed Development is located within the T-D-O Zone and is exempt from the requirements of the tree canopy coverage requirements.

Site Elements / Screening

Standards

Screening materials shall consist of evergreen trees and shrubs, walls, or fences.

Loading areas consisting of off-street loading spaces, loading docks, and service or maintenance areas shall be screened so as to not be visible from an A Street or single-family residential dwelling units.

Comment: Loading docks are contained within the building, where screening is not required.

All mechanical equipment and meters shall be screened to prevent excessive noise and visual impacts on surrounding properties.

Comment: No at-grade mechanical equipment is proposed.

Guidelines

Screening options may include:

- *Six-foot-high, sight-tight, and/or green fences. » Architecturally decorative or green walls.*
- *Evergreen screens (height, spacing, and variety to be determined at the time of Detailed Site Plan based upon the size and location of the area to be screened).*

*All dumpsters; trash pads; trash and recycling collection; or storage areas should be carefully located and oriented on the site to be as inconspicuous as possible.**

**Blank walls should be avoided along pedestrian rights-of-way because they disrupt the street pattern and make an uninviting pedestrian experience; however, if necessary, blank walls should not be more than 10 to 20 feet wide.*

Comment: Screening is not required at the locations mentioned. The only blank wall that exceeds 25' in length is facing Liberty Lane. It is approximately 30' in length and is envisioned as a wall for a local artist mural.

Site Elements / Walls, Fences, and Gates

Standards

The following are permitted:

- *Fences and gates, up to a height of 40 inches, to delineate outdoor seating areas within the retail zone for eating and drinking establishments.*
- *Walls, fences, and/or gates where expressly permitted by this TDDP for screening purposes.*
- *Walls required for the retention of earth, support of a structure, or architectural façade of a structure.*
- *Only in the R-20 and R-80 zones, fences enclosing all or a portion of a rear yard and garden walls; such walls shall not occur forward of the rear of a primary building.*

Comment: A retaining wall at the northeast corner of the site is required to mitigate grade between the existing transformers and the proposed retail terrace. Another retaining wall at the southeast corner of the site is proposed between the building and back of sidewalk to retain grade.

The following are prohibited:

- *Chain link fencing, barbed wire, wire mesh, corrugated metal or fiberglass, split fencing, sheet metal, and paneled materials.*
- *Except where expressly permitted by this TDDP, all other walls, fences, and gates abutting pedestrian or vehicular rights-of-way in the Neighborhood Edge or located anywhere in the Downtown Core.*

Comment: No fences abutting pedestrian or vehicular rights-of-way are proposed.

Guideline

Fence type should be consistent along a block face.

Comment: No fences are proposed in this DSP.

Architectural Elements / Intent

To create attractive and visually interesting buildings that enliven and reinforce streetscapes and public spaces while respecting existing development, including neighboring single-family residential neighborhoods.

Architectural Elements / Signage / General

Standards

All attached or projecting signs are permitted to be placed horizontally or vertically.

Attached signs shall consist of three types: wall signs, window signs, and digital signs, as defined in this section.

Projecting signs shall consist of four types: blade signs, awning signs, high-rise building identification signs, and vertical corner signs.

The following signs are not permitted in the Transit District:

- *Signs not expressly identified in this chapter.*

- *Signs that obstruct any opening intended to provide ingress or egress for any building or structure.*
- *Signs that obstruct the view of traffic control devices.*
- *Signs that, because of their shape, color, or wording, may be confused with any traffic control device (placed by a public authority), or may mislead motorists.*

Comment: The Proposed Development includes wall, blade, corner, identity, and freestanding signage that is permitted in the Transit District and complies with applicable TDDP signage Standards.

ATTACHED SIGNS

Standards

Wall Signs

Signs shall be rectangular and oriented horizontally or vertically.

Vertical wall signs may not exceed a maximum dimension of 36 inches by twice the business' frontage width.

The bottom of a wall sign shall not be installed less than 10 feet above the sidewalk.

A wall sign with digital or electronic content is a Digital Screen.

Comment: Wall signs are proposed for the mixed-use building's façade along Liberty Lane, America Boulevard, and East-West Highway. The proposed wall signs identify the project's name, the Property's address, access/egress areas, amenity area, and the residential lobby area. The proposed wall signs comply with applicable dimension Standards. A leasing/marketing banner is proposed along Belcrest Road.

PROJECTING SIGNS

Standards

Blade Signs

The top of the blade sign shall be between 10 feet and 12 feet above the sidewalk.

The blade sign shall be 36 inches tall maximum.

Blade signs shall be no more than 48 inches wide or project more than 60 inches from the wall. No blade sign shall exceed 6 square feet.

Brackets or other suspension devices shall match the architectural style of the building and shall not be computed as part of the allowable size of the sign.

Comment: Two blade signs are proposed. A building identity mounted blade sign is proposed along the intersection of East-West Highway and America Boulevard, at the southern gateway to University Town Center. Tenant identity blade signage is proposed along America Boulevard, at the northeast corner of the Proposed Development.

OTHER FREESTANDING SIGNS

Standards

Freestanding signs, other than monument signs, single-family detached home and townhome development identification signs, sculptural signs, and A-frame sign boards, shall be located only in the Tree and Furnishing Zone or retail zone within the Downtown Core. Such freestanding signs shall not exceed eight feet in height nor three feet in width.

Comment: An A-frame sign is proposed at the southeast corner of the Property, adjacent to the primary residential lobby. The proposed A-frame sign complies with the applicable Frontage Zone and size Standards.

Architectural Elements / Awnings

Standard

Metal, plastic, and backlit awnings shall not be permitted.

Comment: There are no awnings provided on the project. Metal framed canopies are designed at the main entrance and retail storefront.

Architectural Elements / Balconies

Standard

All balconies, including the addition of balconies to an existing structure, are permitted in the Transit District.

Comment: Prefabricated metal balconies are proposed on the facades along America Boulevard and Liberty Lane.

Architectural Elements / Sustainable Building Materials

Guideline

Buildings should be built with “green” building materials. Green materials should meet the following criteria: produced locally or salvaged; recycled and/or recyclable; rapidly renewable; durable; containing a low embodied energy; manufactured in a less environmentally hazardous or toxic manner; for wood, certified in accordance with the Forest Stewardship Guidelines for environmentally responsible forest management; for refrigerants and fire suppression devices, not containing CFCs or Halon gas. Common green materials include cement and wood fiber composite siding, cellulose insulation, glue-lam beams, and concrete with fly ash content.

Comment: As previously mentioned, the project is pursuing a minimum of a NGBS Bronze certification with a chapter dedicated for Resource Efficiency. This includes the use of recyclable, durable and resource efficient materials as well as locally sourced materials.

Parking and Loading / Intent

To promote a “park once” environment that enables people to conveniently park and access a variety of uses in a pedestrian-friendly environment where streetscapes are vibrant and active and not dominated by parking lots or garages and to encourage nonmotorized modes of travel to and within the Transit District.

Standards

There is no minimum number or ratio of off-street parking spaces for any development within the Transit District.

The maximum number of off-street parking spaces permitted for nonresidential and residential development is specified in the table of maximum parking ratios on the following page. For the

purposes of this table, the type of development refers to its description in the table of uses.

[Table: Maximum Parking Ratios

Downtown Core 1.25 spaces per dwelling unit.]

Comment: A multi-story parking structure will be constructed within the interior of the Property and wrapped by the mixed-use building, providing a total of 272 parking spaces for the Proposed Development. The parking garage will feature Standard 9.5' x 19' parking spaces and 9.5' x 19' ADA spaces with 5'-10' wide embark/debark areas. The proposed 272 parking spaces comply with the applicable minimum and maximum TDDP parking ratios for the Downtown Core.

On-street parking shall be required on all new private A and B Streets constructed pursuant to this TDDP and is encouraged on all County and municipal A and B Streets.

Comment: No new private streets are proposed.

On-street parking in Alleys shall be prohibited.

Comment: No on-street parking in alleys is proposed.

Development may only be permitted to exceed the maximum parking ratios if all of the following criteria are met:

Additional parking spaces may only be provided in the form of structured parking.

[Standards following for surface parking and total permissible number of off-street parking spaces in the Transit District are not reproduced here.]

Comment: The subject Detailed Site Plan is not proposing any off-street parking spaces.

All applicants, other than those proposing solely single-family dwelling units, shall demonstrate the extent to which their proposed development reduces the total number of surface parking spaces within the Transit District.

Comment: The subject Detailed Site Plan replaces an existing surface parking lot with a mixed-use transit-supportive community. Accordingly, the Proposed

Development reduces the total number of surface parking spaces within the Transit District.

All new structured parking facilities shall include secure bicycle parking. One bicycle parking space shall be provided for every 10,000 square feet of building area for office, retail, hospitality, and other commercial, public, and institutional uses. One bicycle parking space shall be required for every 20 units for multifamily residential development. These bicycle parking requirements are cumulative for mixed-use development, and both open and covered bicycle parking areas may be provided, as appropriate.

Comment: The development provides 97 bicycle parking spaces within the building and 8 bicycle parking spaces on-site.

Parking may be located on- or off-site within one-quarter mile walk of the development site. When off-site parking is used to meet any parking needs, the applicant shall provide a site plan and narrative statement demonstrating that parking is provided off-site and that pedestrian facilities necessary to serve the walk from the parking facility to the building will be constructed prior to the opening of the parking facility.

Comment: No off-site parking is proposed or will be required to satisfy the Proposed Development's parking needs.

Carpool and vanpool parking spaces shall be required at a minimum ratio of one reserved high occupancy vehicle space per every 100 regular parking spaces for any development including in excess of 50,000 square feet of office use. Free or reduced parking costs for authorized carpools and vanpools are encouraged.

Comment: The Proposed Development does not include office uses.

Restriping of surface parking facilities that result in an addition of general purpose parking spaces is prohibited.

Comment: Existing surface parking at the Property will be redeveloped and replaced by the Proposed Development.

On-street parking spaces on private streets shall not count toward off-street parking requirements.

Comment: No on-street parking is proposed with this DSP. Existing on-street parking will remain, but will not count toward off-street parking requirements.

All parking for buildings that front on Adelphi Road shall not be visible from Adelphi Road.

Comment: No part of the subject property fronts on Adelphi Road.

Guidelines

Commercial parking facilities should leave at least 25 percent of their spaces available for hourly and daily rental by the general public.

Comment: Existing on street parking is provided for the retail use.

All parking spaces that are provided should be unbundled from the leasing and/or rental rates of associated development.

Comment: All parking spaces will be unbundled from the leasing and/or rental rates of the proposed multifamily development.

Parking and Loading / Surface Parking

[No surface parking is proposed. Accordingly, Surface Parking Standards are not included in this analysis.]

Parking and Loading / Structured Parking

Standards

A structured parking garage shall not front onto MD 410 (East-West Highway) or Belcrest Road at ground level.

Comment: The multi-story parking structure will be constructed within the interior of the Property and wrapped by the mixed-use building and does not front onto East-West Highway.

On-site, off-street parking for buildings located along MD 410 (East-West Highway) or Belcrest Road shall be located behind, below, or above those buildings.

Comment: The multi-story parking structure will be constructed within the interior of the Property and wrapped by the mixed-use building. The parking structure entrance is located on Liberty Lane.

Vehicular entrances to, and exists from, parking structures shall not be located on A Streets.

Comment: Vehicular entrances and exits are located on Liberty Lane, a B street.

Where otherwise permitted, parking garages that front directly on a street shall be provided with architectural treatment, liner uses and buildings, murals, landscape or any other screening techniques in order to screen the garage from the street.

Internal elements such as pipes, fans, and lighting shall be concealed from public view. Where possible, ramping should be internalized.

Comment: The multi-story parking structure will be constructed within the interior of the Property and wrapped by the mixed-use building. Structured parking does not directly front adjacent streets.

Guidelines

Parking structures should be designed as an integral component of the coordinated site plan and architectural theme.

Garages should use articulation or fenestration treatments that break up the massing of the garage and/or add visual interest.

When designing and constructing above-ground parking structures, consideration should be given to the installation of renewable energy systems (such as solar photovoltaic panels or wind generators or green roofs on or above their top deck).

Comment: The project proposes a 6-story parking garage, with access from Liberty Lane, wrapped by residential building and is only exposed at the west property line to the adjacent development on Parcel H.

Parking and Loading / Loading

Standards

There is no minimum number of off-street loading spaces in the Transit District.

Comment: Loading and servicing for the project have been strategically located to minimize pedestrian vehicular conflicts. Loading will be provided internal to the site and under the building with access from Liberty Lane.

The required number of off-street loading spaces shall be determined at the time of DSP.

Comment: The building provides one off-street loading space.

Off-street loading spaces may only be located in the rear of buildings.

Comment: The project's internal loading space is located on Liberty Lane.

Guidelines

The provision of on-street loading zones is encouraged; such zones should be strategically located to serve multiple business and time-limited to ensure turnover.

The required number of off-street loading spaces should not exceed the minimum required by Section 27-582 of the Zoning Ordinance.

Comment: The required number of proposed off-street loading spaces does not exceed the minimum required by the Zoning Ordinance.

Transportation Adequacy / General

Standards

Within the Prince George's Plaza Transit District, properties are required to demonstrate that the transportation facilities (limited to only signalized and/or unsignalized intersections) will be adequate to serve the proposed development for any preliminary plan, and any DSP if prior adequacy findings are more than six years old.

The adequate public facilities (APF) finding must follow and conform to the procedures and methodologies outlined in the most recent edition of the Planning Board's Transportation Review Guidelines, Part 1, unless as specified in this section. Within the Prince George's Plaza Transit District, the transportation

facilities adequacy standard shall be LOS E for individual critical intersections.

Comment: Adequacy of public transportation facilities was determined at the time of Preliminary Plan of Subdivision.

Determination of Intersections to Be Studied

The number of intersections that shall be included in the required adequacy analysis will be based on the total projected total weekday vehicle trips generated by the proposed development under consideration. Table 48 on page 295 shows the number of signalized and/or unsignalized intersections within the Transit District required to be included in the adequacy analysis. The Planning Director, in consultation with the County and/or appropriate municipality, may adjust the number of intersections required for analysis if he/she finds that special circumstances warrant a different number of intersections to be scoped for adequacy determination.

[Table: Maximum number of Intersections to be Studied:

Proposed Vehicle Trips:

Comment: Determination of intersections was made at the time of Preliminary Plan of Subdivision.

Required Improvements

For all the required intersections, the applicant shall demonstrate one of the following:

- 1. There will be adequate intersection level of service, as defined by Section 24-124(a), provided that the intersection(s) are fully built to the plan's ultimate configuration.*
- 2. Applicant is willing to fully fund and implement trip reduction program(s), deemed appropriate by the County DPW&T for a period of at least six years after the issuance of first occupancy permit that would alleviate any reported inadequacy as defined by the Transportation Review Guidelines.*
- 3. The applicant agrees to make a one-time Prince George's TDDP Transportation Mitigation Payment to the County and for each signalized intersection that is projected to operate above the*

acceptable LOS, provided the intersection(s) are fully built to the plan's recommended configurations.

Comment: Adequacy of public transportation facilities was determined at the time of Preliminary Plan of Subdivision.

Site access adequacy and needs shall be determined by the appropriate operating agency having jurisdiction over the roadway where the access point(s) are to be located.

Comment: Site access adequacy and needs will be reviewed by the Department of Permitting, Inspections and Enforcement on behalf of the City of Hyattsville.

Calculation of TDDP Transportation Mitigation Payment

The required one-time transportation mitigation fee for each required signalized intersection that is operating above the minimum acceptable clear lane volume (CLV) of 1,600 vehicles per hour, or as modified by the Guidelines, to be paid to the County prior to issuance of any building permit shall be the smaller of one of the following:

- a. \$10,000 per unit of CLV over the Acceptable Value of 1,600 critical vehicle volume.*
- b. \$100 per projected daily weekday vehicle trip for the proposed development.*

Allocation of the Collected TDDP Transportation Mitigation Fees

On an annual basis, the Prince George's County Planning Board and/or the City of Hyattsville, can request that the County appropriate any portion or all of the collected funds to any transportation enhancement projects, excluding capacity enhancements to existing roads, within, and projected to serve, the Transit District

Comment: This Standard is acknowledged.

Downtown Core Standards / Intent

To create a compact, walkable, mixed-use regional destination whose built environment promotes a sense of place and include multimodal transportation access, lively streetscapes, and active public spaces.

DOWNTOWN CORE GENERAL

Standards

Accessory buildings are not permitted in the Downtown Core. Accessory, subordinate, and services uses should be housed within the main building or attached parking structure.

Comment: No accessory buildings are proposed in this DSP.

There are no lot coverage or minimum green area requirements for the development within the Downtown Core.

Comment: This Standard is acknowledged.

Guideline

On private streets in the Downtown Core, all pedestrian crosswalks should be a minimum of 12 feet wide.

Comment: The Standard is acknowledged. There are no proposed crosswalks across private streets.

All public utilities should be placed underground, above-ground in alleys, or otherwise behind structures.

Comment: All public utilities serving the Proposed Development will be placed underground, above-ground in alleys, or otherwise behind structures, in conformance with the TDDP.

DOWNTOWN CORE BLOCKS

Standards

Primary facades shall face A Streets, Pedestrian Streets, or Promenades.

Comment: The design of the East-West Highway & America Boulevard corner was developed to serve as a gateway to the southern entrance of University Town Center and to address the significance of this intersection to the built pedestrian environment at East-West Highway. This gateway is accomplished by creating the main entrance and residential amenity at the corner, connecting to the existing

sidewalks, and emphasizing the entrance through various architectural design strategies. These strategies include the articulation of building forms, the simple use of materials, increased transparency at the corner and the breaking down of the scale and proportions of the building. The design also embraces the natural slope of the site, which features a low point along East-West Highway at the intersection with America Boulevard, revealing the entrance with extensive corner glazing extending along America Boulevard and East-West Highway. The corner entrance is also highlighted with a canopy, and local mural artwork. The building features and materials are also highlighted at the corner using a unique balcony on the upper floors and the transitioning of exterior materials to articulate the corner.

Guidelines

Buildings should occupy the perimeter of a block

Comment: The mixed-use building addresses the varied topography of the site while complimenting the walkability and connectivity to University Town Center and the adjacent neighborhood by designing up to build-to-lines and providing an edge along the Property.

Blocks should range in size from 200 to 400 linear feet on each side.

Comment: This guideline is acknowledged. The block along East-West Highway is 248', the block along America Boulevard is 267', and the block along Belcrest Road is 130' in length. America Boulevard and Liberty Lane is 459' in length.

DOWNTOWN CORE A STREET

Standards

The following streets existing in the Downtown Core on July 19, 2016, are to be considered A Streets:

- *MD 410 (East-West Highway)*
- *Belcrest Road*
- *Toledo Terrace*
- *America Boulevard*
- *Toledo Road*

Primary entrances to new buildings shall be located on A Streets.

Comment: The primary building lobby will be located on the corner of East-West Highway and America Boulevard.

No service area or loading dock shall be permitted adjacent to any street unless enclosed such that all sides appear as a primary façade similar to the primary façade of the main building, including design, detail, finished material, and landscaping.

Comment: The proposed loading dock entrance is located on Liberty Lane (B Street).

DOWNTOWN CORE B STREET

Standards

Streets existing in the Downtown Core on July 19, 2016, not identified as A Streets are to be considered B Streets.

Delivery services, loading, dumpsters, parking facility (surface and structured) entrances, and above-ground utilities servicing buildings fronting on A Streets or Pedestrian Streets shall be located on B Streets or Alleys.

Comment: The subject DSP complies with the Downtown Core B Street Standards, as these services and facilities will be located along Liberty Lane.

Guidelines

Secondary building entrances may be located on these streets.

Comment: Secondary building entrances are proposed on Liberty Lane.

DOWNTOWN CORE PEDESTRIAN STREETS AND PROMENADE

Standards

Access to a Promenade shall be limited to bicycles and pedestrians. Vehicular access is prohibited on promenades, except emergency and service vehicles, and other temporary access as permitted by DPW&T or the City of Hyattsville, as applicable.

Comment: This Standard is not applicable, as the subject DSP does not include a Promenade.

Service and loading entrances are not permitted on Pedestrian Streets or Promenades.

Comment: This Standard is not applicable, as the subject DSP does not include Pedestrian Streets or Promenades.

DOWNTOWN CORE FENESTRATION

Standards

Facades at the ground level facing A Streets, Pedestrian, Promenade, or fronting an open space shall be visually permeable (clear glass windows, doors, etc.); at a minimum, 50 percent of the ground floor façade shall consist of transparent materials (glass).

Comment: The design reveals the entrance with extensive corner glazing extending along America Boulevard and East-West Highway and a retail use at the corner of America Boulevard and Liberty Lane at the northeast corner of the development. The corner entrance is also highlighted with a canopy, and local mural artwork.

Tinted or mirrored glass, or glass blocks are not permitted.

Comment: Mirrored glazing or glass block will not be provided. Slightly tinted glazing may be required to meet energy performance requirements.

Guideline

Fenestration should be a minimum of 70 percent of the first-floor facades along A Streets. On B Streets where retail is the first floor use, the minimum percentage should be 60 percent of the ground floor façade.

Comment: The design of the building provides 31% fenestration on East-West Highway (A Street), 45% on America Boulevard (A Street) and 38% on the corner of

Liberty Lane (B Street). The design provides high glazing fenestration at the residential lobby and retail corner to provide an open streetscape experience.

DOWNTOWN CORE MIXED-USE AND NON RESIDENTIAL BUILDINGS

Standards

All buildings on abutting lots within the Downtown Core shall be attached at the sides except where separated by a street (as defined by this TDDP), plaza, or public open space.

Comment: This Standard is not applicable to the Proposed Development, which is located on a single parcel within the Downtown Core. Moreover, an existing commercial office building is located on abutting Parcel H. However the building is setback from the abutting property line with Parcel N and fronts the intersection of East-West Highway and Belcrest Road.

The minimum clear height of retail space and of storefront fenestration is 14 feet.

Comment: The design provides high ceiling spaces at the residential lobby and retail corner to provide an open streetscape experience. A lower, 9' ceiling height is proposed for the residential units.

Retail awnings may encroach across the BTL for a maximum of six feet. Marquees, fixed canopies, and all other permitted overhead entry features may encroach beyond the BTL for more than six feet maximum but may not project beyond the Tree and Furnishing Zone.

Comment: The proposed fixed canopies comply with this Standard.

Guidelines

Components of ground floor frontage for commercial or other nonresidential uses should support an active streetscape.

Comment: The retail space at the northeast corner of the project has an opportunity to spill out onto the retail terrace at the corner, as well as the sidewalk. The lobby and amenity spaces have enhanced paving for additional interest.

All buildings located at the intersections should incorporate landmark or/and architectural elements (sculpture, unique building shapes, interesting façade articulations, etc.) to highlight intersections as the entry points to the Transit District.

Comment: The design of the East-West Highway & America Boulevard corner was developed to as a gateway entry point to the southern entrance of University Town Center. This gateway is accomplished by creating the main entrance and residential amenity at the corner, connecting to the existing sidewalks, and emphasizing the entrance through various architectural design strategies. These strategies include the articulation of building forms, the simple use of materials, increased transparency at the corner and the breaking down of the scale and proportions of the building. The corner entrance is also highlighted with a canopy, and local mural artwork. The building features and materials are also highlighted at the corner using a unique balcony on the upper floors, and the transitioning of exterior materials to articulate the corner.

All street-defining buildings on corners should orient main entrances toward the corner.

Comment: The design locates the residential lobby at the corner of East-West Highway and America Boulevard. Retail use is provided along the northeast corner at the intersection of America Boulevard and Liberty Lane.

First floor balconies should be recessed from the main building façade, or three feet higher than ground elevation.

Comment: The development does not have any balconies on the exterior first floor.

Overhead features such as awnings and fixed-canopies should be spaced along a frontage at 75-foot intervals maximum to encourage active streetscape. Tighter spacing is encouraged.

Comment: This guideline is acknowledged.

[Figure 27. Downtown Core Multistory: Commercial, Residential, Institutional Standards]

Building Placement

Build-to Line Regulated by Street Section

BTL Defined by a Building

<i>Front (Primary Street):</i>	<i>80% minimum A Street, Pedestrian Street, or Promenade</i>
	<i>60% minimum B Street</i>
<i>Side:</i>	<i>80% minimum</i>
<i>Side (Side Street):</i>	<i>100% minimum A Street, Pedestrian Street, or Promenade</i>
	<i>60% minimum B Street</i>
<i>Zones Permitted:</i>	<i>M-U-I/M-X-T</i>

- 1. A building form with a chamfered corner is allowed only on corner lots and only if a corner entry is provided.*
- 2. No planting strips allowed between sidewalk and building unless specified on street sections.*
- 3. In front and side yards where buildings do not meet the build-to line, only public open spaces, plazas, or seating for eating and drinking establishments are permitted.*

Building Form

Height: *See Density and Building Height section.*

Ground Floor Ceiling *14' minimum clearance.*

- 4. Any buildings wider than 50' shall be designed to be seen as a series of building fronts no wider than 50' each.*
- 5. Loading docks, overhead doors, and other service entries shall not be located on A Streets, Pedestrian Streets, or Promenades.*

Comment: Planting strips between the building and back of sidewalk have been provided in order to buffer units from public activity. Where retail and entrances exist, no strip is provided.





SOUTH ELEVATION - EAST-WEST HIGHWAY



EAST ELEVATION - AMERICA BOULEVARD



NORTH ELEVATION - LIBERTY LANE - EAST



NORTH ELEVATION - LIBERTY LANE - WEST



WEST ELEVATION - BELCREST ROAD



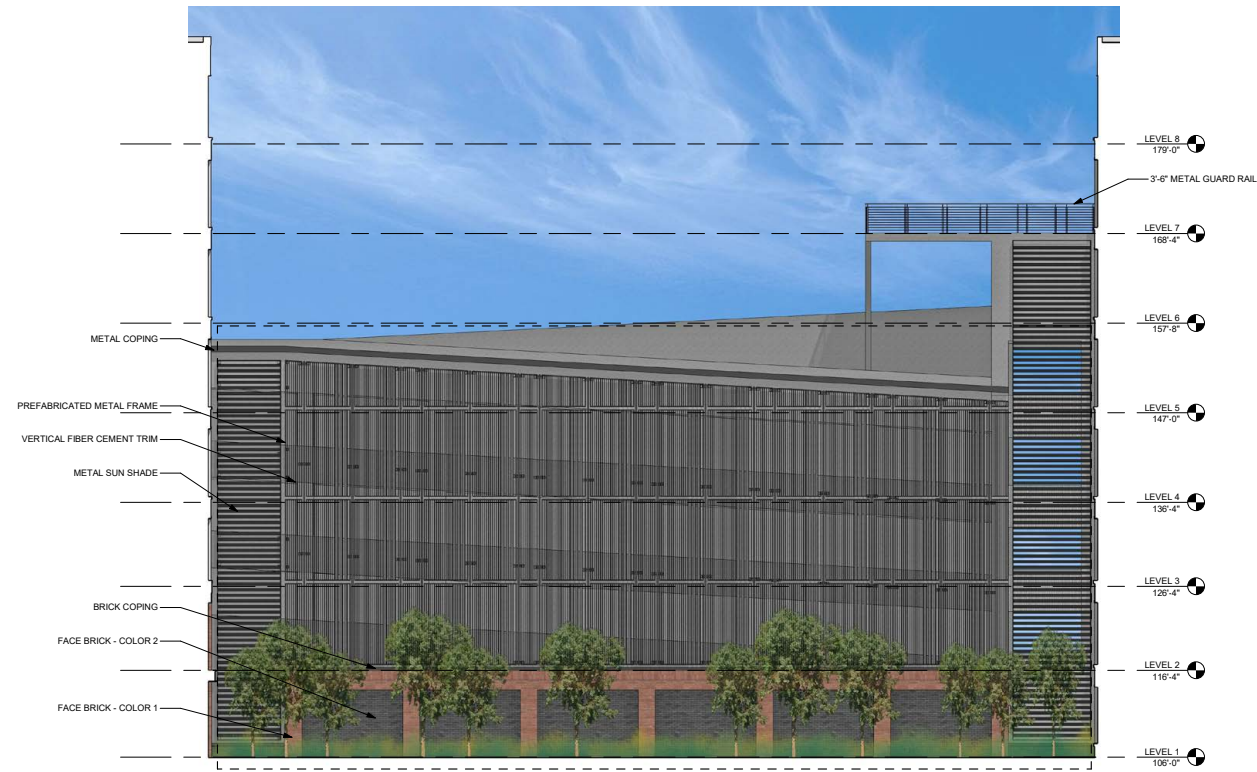
SOUTH ELEVATION - PROPERTY LINE - WEST



SOUTH ELEVATION - PROPERTY LINE - EAST



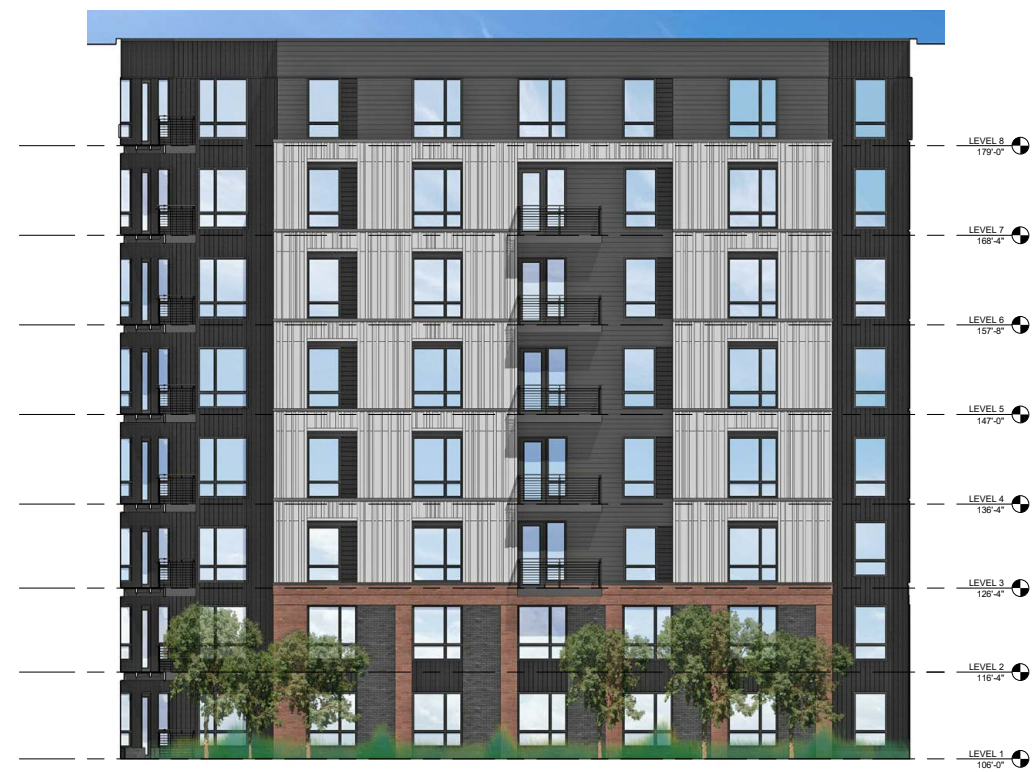
WEST ELEVATION - PROPERTY LINE - SOUTH



COURTYARD WEST ELEVATION



COURTYARD NORTH ELEVATION



COURTYARD EAST ELEVATION



COURTYARD SOUTH ELEVATION













